

Eastern City Public Realm Vision



EASTERN CITY

Publica

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Nick Carty,
EC BID Board Chair

The Eastern City tower cluster is an extraordinary commercial hub, supporting over 90,000 jobs with an economic output in excess of £11bn per year. It's a bustling, vibrant microcosm of London and a global exemplar, rich in history, commerce, and opportunity. Of course, traditionally home to financial and insurance businesses, which make up around 40% of jobs in the area, the Eastern City is evolving, as it has done for the last 2000 years.

With city worker numbers rising, the City Corporation is negotiating development proposals that would contribute to providing a projected demand of 1.2 million square metres of Grade A office space, by 2040, equivalent to roughly 150 football pitches, with hundreds of thousands of square metres already approved and under construction. Much of this growth will happen in the Eastern City's tall building cluster area, where 11 new tall buildings are underway or have the green light. These include 1 Leadenhall Street, 100 Leadenhall Street, 40 Leadenhall Street, 70 Gracechurch Street, 55 Old Broad Street, 99 Bishopsgate, and 55 Bishopsgate.

This report, and the various strategies and policies it supports, recognises the importance of the spaces we use on a daily basis – the 'spaces between the buildings', the green pockets, the seating areas, the courtyards, alleyways and routes we use to move around the City. Spaces matter. In the wake of a continuing adjustment post pandemic and rising global competition, the quality of the City's Eastern Cluster offer is more important than ever.

The City of London Corporation's flagship Destination City programme and planned investments make a significant contribution to reinforcing the exemplary reputation of the Square Mile and highlight the significant opportunities to attract different audiences and demographics to the area.

Within this context, this report commissioned by the EC BID proposes a future Vision for the Eastern City which I hope is one that all partners can coalesce around. Through a new partnership approach, bringing together the City of London Corporation, other statutory partners, property developers and building occupiers, we are seeking to maximise the opportunities related to the growth in the area, and help enhance the experience of visiting and working in the Eastern City.

The purpose of this Vision is to add value to the extensive work that is already underway across the Eastern City by the City of London Corporation, other statutory partners, and the private sector. It aims to build on this work, identify opportunities to make further enhancements and build consensus about the future potential of public realm in the area. Through targeted improvements to public realm and publicly accessible private space, adopting a holistic, area-wide approach to interventions and how these are funded and delivered, we believe the area is on the cusp of an exciting era of collaboration and transformation.

The proposals in this Vision represent possible visualisations and opportunities, not definitive designs. They have been developed in consultation with many stakeholders but have yet to be fully tested. Each of the projects included in the Vision are backed by insight and data but would need to be considered by the local authority, and depending on the scale of the project, will require full consultation, feasibility analysis, drawn briefs, planning consideration and funding agreed. Some of the smaller projects are opportunities that the BID can progress immediately and fund independently, and I look forward to us making these investments in the coming months and years. On the larger projects, the BID stands ready to play an important convening role, encouraging partners to coalesce around shared goals, contributing to and investing in the delivery of some of the transformations being proposed.

This is an important milestone and is a positive statement of intent for how we can work together and deliver more – a new blueprint for partnership that can expedite implementation and formulate innovative funding models rooted in collaboration.

The EC BID would like to thank all stakeholders who contributed information, comments and design suggestions that helped in the development of this Vision and we look forward to turning collective will into collective action in the coming years.



The City of London Corporation welcomes the Eastern City BID's Public Realm Vision and acknowledges its value in adding to our ongoing delivery of the City Cluster Vision. It has provided inspiration and new ideas that align with our strategies for the future prosperity of the Square Mile, including City Plan 2040, the Transport Strategy and the Climate Action Strategy, as well as our flagship Destination City policy.

We recognise the ambition of the Businesses and workers within the Eastern City tower cluster—as this area continues to evolve with significant developments, the Vision offers valuable ideas for enhancing the public spaces that connect and enrich these thriving environments.

Through genuine engagement with the City of London Corporation and the City's diverse stakeholders, the EC BID has demonstrated a commitment to fostering a more dynamic inclusive, safer, cleaner, and greener public realm for residents, businesses, and visitors alike.

We support the Eastern City BID's, and other City Business Improvement Districts efforts and look forward to their continued contributions to the City's growth.

Executive Summary



The Eastern City's density and its identity as a hub of commerce put the area on the map of truly global neighbourhoods. The modern energy of the area stands in contrast to the remnants of the Roman city, which contributes to a distinctiveness that sets the City apart from its international peers.

Why public realm and why now?

More workers, more visitors

More workers are returning to the office, with more joining them as various development schemes reach completion. Through the Corporation's Destination City programme, and with increasing attention given on supporting cultural uses, more visitors, both international and local, will come to the City for non-work activities.

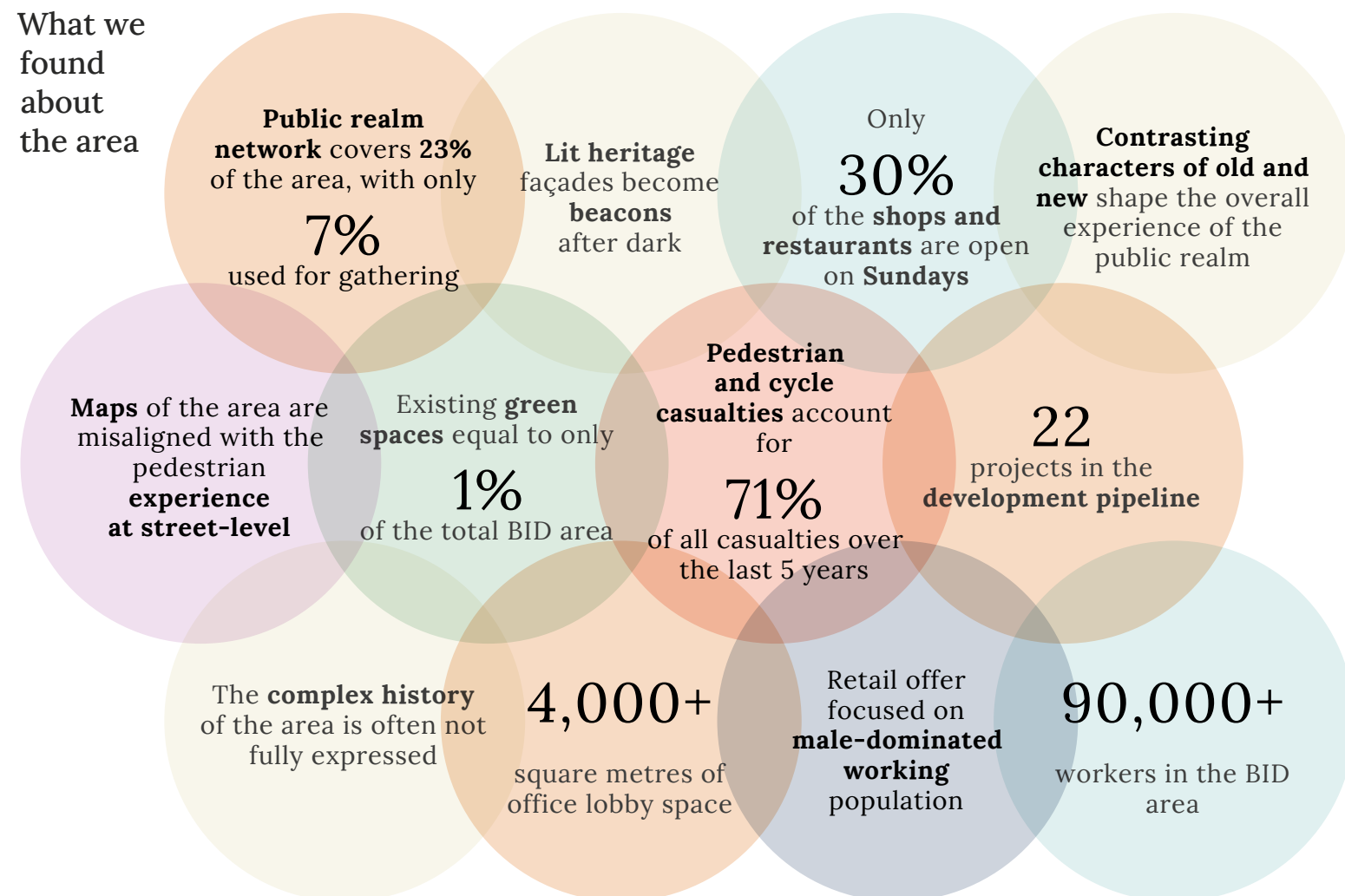
An evolving street network

The City of London Corporation and Transport for London are undertaking projects that will decongest the streets and meaningfully transform public space across the BID area. This Vision complements these ongoing plans and projects, connecting the dots, and filling in the gaps.

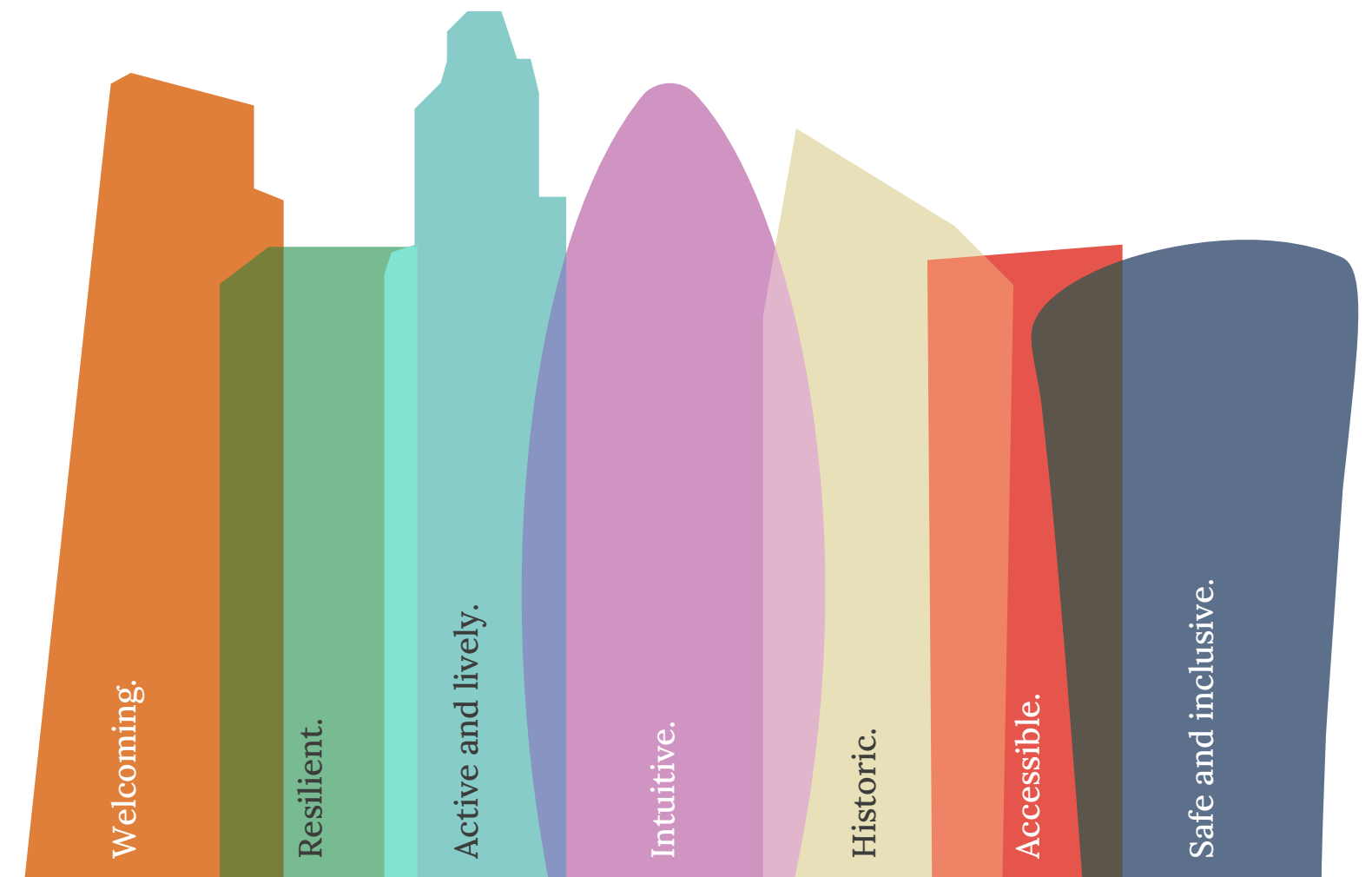
Alignment with business and environmental priorities

Addressing the climate emergency is the most significant global challenge of our time. The area will need to strike a balance between the growth and the need to significantly change behaviours to meet net zero goals. The public realm must play a role in mitigating the risks of a changing climate. This Vision supports the BID's priorities and contributes to ESG goals by creating an inclusive and welcoming place.

What we found about the area



We envision a future public realm of the Eastern City that is:



Welcoming spaces

Design publicly-accessible spaces that meet the needs of those who work and spend time in, live near, and pass through the area.

Intuitive navigation

Make it easier to find your way around the area, while also wandering and discovering.

Resilient trees and planting

Embrace planting as a key way to address net zero carbon goals and climate adaptation, and ensure the area is as green as it can be through protection of existing planting, innovative techniques that are suited to the area's unique microclimate conditions.

Historic character and lighting

Celebrate, reveal, highlight and improve the setting of the heritage features across the area.

An accessible environment for all

Establish environments where all modes of movement can coexist – with priority for walking, wheeling and cycling to contribute to climate and health goals.

Lively uses and activity

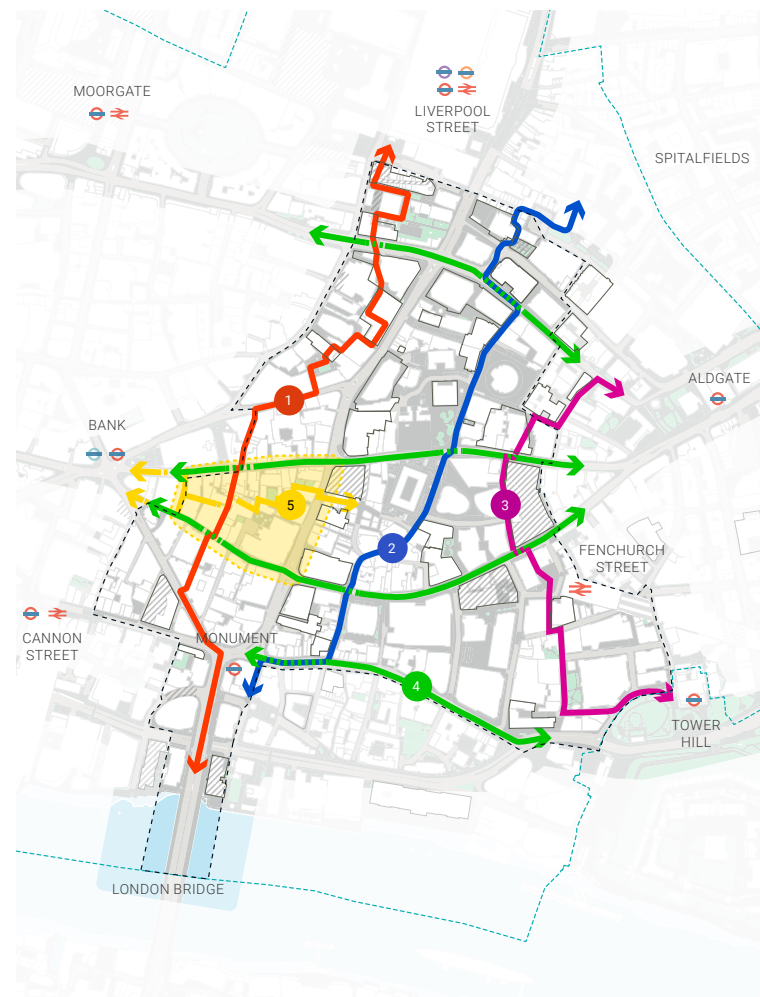
Create a lively place, harnessing the energy of commerce and trade that characterised the area in the past and celebrating the area's cultural and recreational destinations.

A safe and inclusive neighbourhood

Promote streets and spaces where all users feel safe and share in a sense of belonging.

Each of these seven aims is supported by a set of guiding principles and ideas for specific projects and actions the BID could take in the coming years to support the Vision. The aims were established following rigorous research, surveying of the area and conversations with the wider public and stakeholders.

Proposed public realm improvements across the area



- + Proposed physical interventions are grouped along routes through the Eastern City.
- + Three types of improvements, ranging from small-scale interventions, priority projects, and transformative projects.
- + Improvements align with the seven aims of the Vision and address the context and conditions of each route.

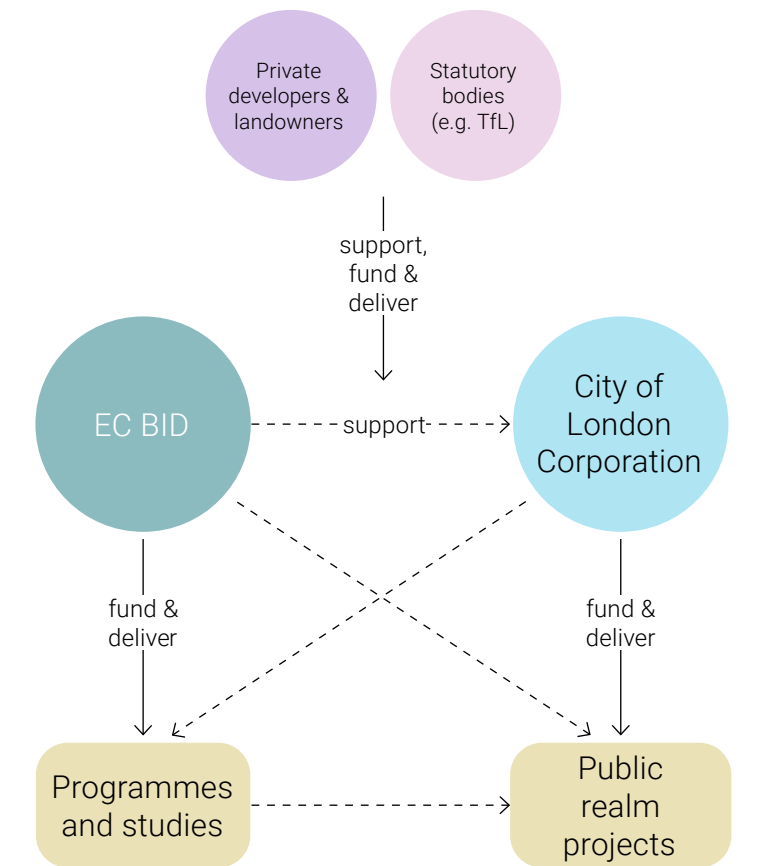
- 1 London Bridge to Liverpool Street
- 2 Monument to Spitalfields
- 3 Tower Hill to Aldgate
- 4 West to East
- 5 Bank to Leadenhall Market

Priority Projects have been selected based on what is already being funded or delivered by the City or by private developers, and through an analysis of those interventions that would have a high-impact on the quality of the public realm across the BID area.

What role can the EC BID play in progressing this Vision?

Partner.
Collaborate.
Share knowledge.
Convene.
Support.

- + This Public Realm Vision is the first step of a long-term process of collaboration and partnership.
- + The BID can implement small-scale initiatives throughout the area that will improve the sense of place in the Eastern City, and fund studies that can lay the groundwork for future public realm improvements delivered by others. Projects of this scale are highlighted throughout the Vision.
- + Larger-scale public realm improvements, such as those highlighted as Priority Projects along the Five Routes, will require ongoing collaboration and partnership across the City of London and landowners.
- + In the coming months, the BID will work to identify short-term projects to implement, as well as discuss the longer-term transformative projects with public sector partners.



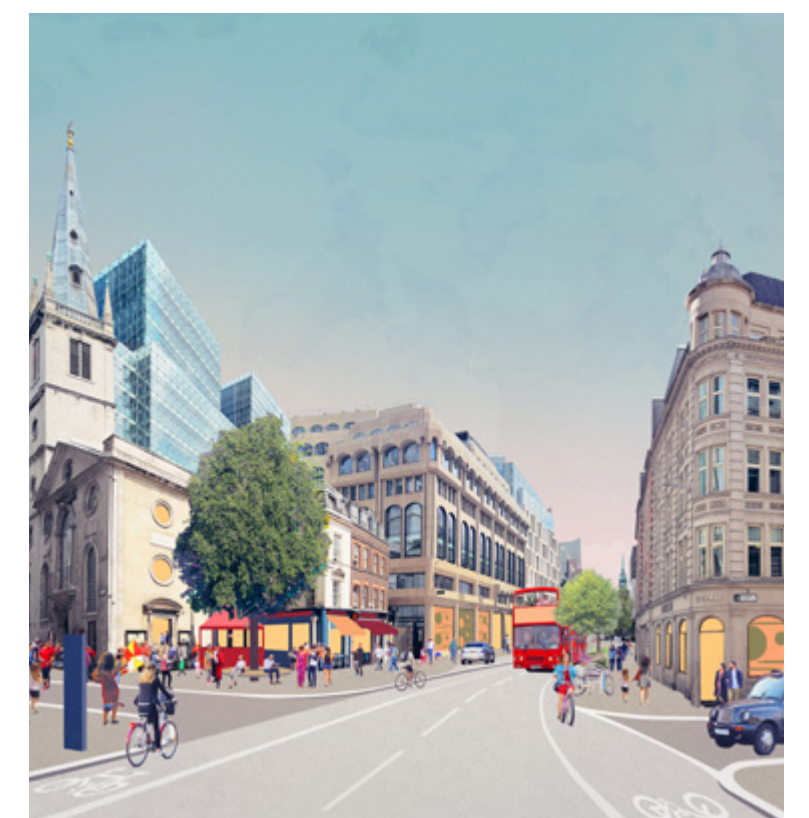
Lombard Street at Birchin Lane
Celebrating heritage



Fenchurch Street Station
A safe and inviting gateway



Camomile Street
A new park in the City of London



Eastcheap
A flexible thoroughfare

Context

A uniquely distinct and exciting place

The area's physical density and its identity as a hub of finance, insurance, and commerce put the Eastern City on the map of truly global neighbourhoods. The modern, international energy of the area, made manifest by glass and steel skyscrapers, stands in contrast to the brick and stone that comprise the remnants of the Roman city, the narrow alleyways and the hidden gardens and yards, all of which contribute to a distinctiveness that sets the City apart from its international peers.

These contrasting characters of old and new have a strong presence on the urban pattern and shape the overall experience of the public realm. Medieval yards and laneways to the west of Gracechurch Street and Bishopsgate have found their contemporary equivalent in the wider covered walkways and large plazas to the east.

The Vision celebrates this contrast as the defining characteristic of the area. The Eastern City feels like an exciting place to be a part of precisely because of its dramatic contrasts, between the commercial activity and towering architecture, and the specificity of its history, smaller-scale buildings, and its quieter, surprising moments.

Why public realm and why now?

Public realm is a foundational part of our cities; serving not only logistical function in connecting us, but also an emotional one in allowing people to connect with each other. A well-considered public realm contributes to better air quality, improved mental health, increased biodiversity and makes a place more welcoming, providing space for all to thrive.

The EC BID was established by a ballot of local businesses in 2022, and the quality of public realm was identified as an early priority. In the context of the Eastern City, publicly-accessible spaces play a vital role. A thriving public realm is as crucial to a growing and innovative business district as it is to a leisure and visitor destination.

There is already significant resource being leveraged toward improving the streets and spaces in this area, by the City of London Corporation, Transport for London (TfL) and private developers. These projects will greatly improve the public realm across the Eastern City; but they present a challenge in ensuring that the public realm remains cohesive. The BID can play a role in supporting the projects that are already underway, and also in convening stakeholders, partnering with the City and developers to deliver the projects that contribute meaningfully to public life.

As of summer 2024, there are 22 projects in the development pipeline, representing hundreds of thousands of square meters of new office space. The significant development pipeline is bringing more office workers to the area in the coming years, alongside the growing Destination City agenda encouraging an increasing number of leisure visitors. This growth is welcome and will support the evolution of a vibrant and successful place. Alongside this however, the growth will undoubtedly put pressure on an already squeezed public realm.

Global cities face many challenges, from responding to the climate crisis and meaningfully engaging on ESG, to remaining competitive and relevant in the post pandemic environment. As the Eastern City area stands on the edge of a new era of transformation and growth, now is a good time to consider what opportunities these changes present for public realm, helping to create a more accessible, inclusive and liveable destination.

More workers are returning to the City on more days across the week, with an average of 2.7 days per week, an increase of nearly 20% in the past year.

The benefits of the small open spaces across the Square Mile have been estimated to be worth £126.8 million every year, totalling over £3 billion over the next 50 years.



Context

A dynamic development pipeline

33,000 square metres of publicly-accessible open space at street level—an area equal to around 3 football pitches.

2024 is a fitting year to be articulating this Vision, given the amount of growth underway. In alignment with the draft City Plan 2040 and the priorities set out in the BID's business plan, there are hundreds of thousands of square metres of new office space currently in planning and pre-planning stages across the BID area.

This amount of development will welcome thousands of new workers to the area, putting stress on an already-pressured public realm. If all the office development that is currently in the pipeline is approved and built over the next 15 years, pavements will be more crowded at peak commute times and at lunchtime outdoor spaces, especially those that get slivers of sunlight through the tall towers, will be in higher demand. These new workers will also require different types of services, shops, activities, and interventions that suit their needs.

More workers, more visitors

As the City sees more workers returning to work, the Corporation is promoting cultural and leisure initiatives to bring activities and events to the City that extend beyond working hours. New uses, and a more varied array of things to do and see are all part of the vision for the City to attract a more diverse audience.

Through the Destination City programme, and with increasing attention given on supporting cultural uses, more visitors, both international and local, will come to the City for non-work activities.

An evolving street network

4,900 square metres of green space—an area equal to around half of a football pitch.

The City of London Corporation and Transport for London (TfL) are undertaking projects that will decongest the pavements and streets and meaningfully transform public space across the BID area.

TfL's traffic schemes include improvements to Monument Junction, Gracechurch Street, while the Corporation's portfolio of projects include the London Wall Corridor, Jubilee Gardens, Cornhill, King William Street, Leadenhall Street, and Healthy Streets Plans.

This Vision complements these ongoing plans and projects, and seeks to connect the dots, fill in the gaps between these various studies, and ensure that every square metre of space is performing at its best.

Alignment with business and environmental priorities

London will be increasingly affected by temperatures of over 40°C, bringing stress to the electrical grid, stronger storms, and stormwater floods. At the same time, London's summertime water supply will become more precarious, increasing the risk of drought and fire. (GLA, London Climate Resilience Review, 2024)

Addressing the climate emergency is the most significant global challenge of our time. The City of London will need to strike a balance of the growth-oriented priorities with the need to significantly change behaviours to meet net zero goals. The Corporation's commitments to net zero have been outlined in its Climate Action Strategy, which the BID fully supports. The public realm is both affected by the changing climate, and must play a role in helping to mitigate climate change effects at street level while supporting active travel.

Efforts should be made in public realm design to meet net zero goals, such as considering circular approaches to materials, using low-carbon materials and supply chains, incorporating planting that is resilient and biodiverse, and nature-based solutions, like sustainable drainage systems (SuDS), that address higher flood risk.

Pedestrian, transport and cyclist-friendly street improvements can lower the dependence on cars, and freight, servicing and delivery (FSD) consolidation can reduce the number of vehicle trips, simultaneously improving air quality.

This Public Realm Vision underpinned by the seven strategic aims supports the Corporation's net zero targets, and encourages positive climate action across the BID area.

Context

fig. 1 Buildings in the development pipeline

- Pre-planning
- In planning
- Consented
- In construction
- Recently completed

See Evidence Base for full list of project in the development pipeline

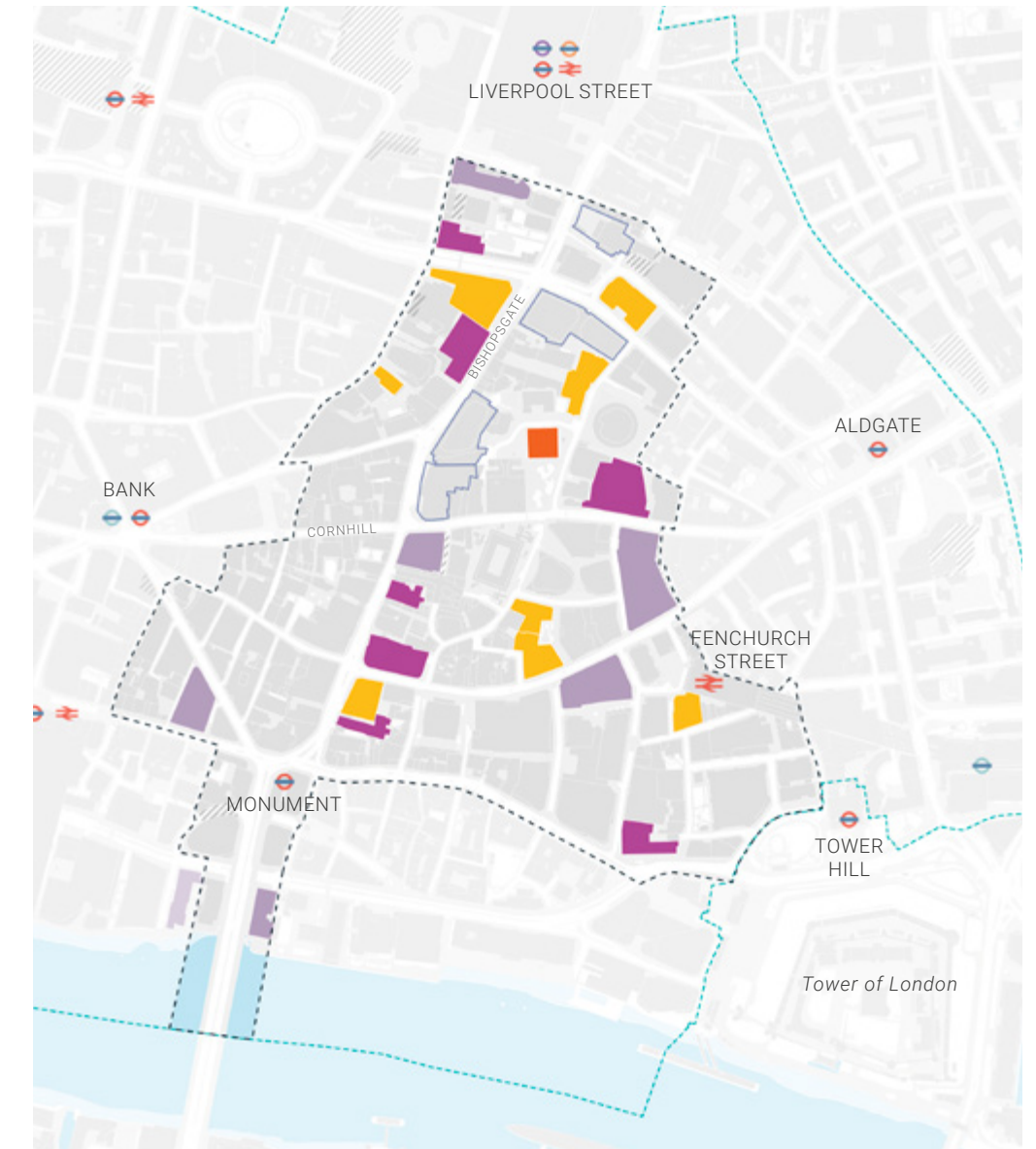
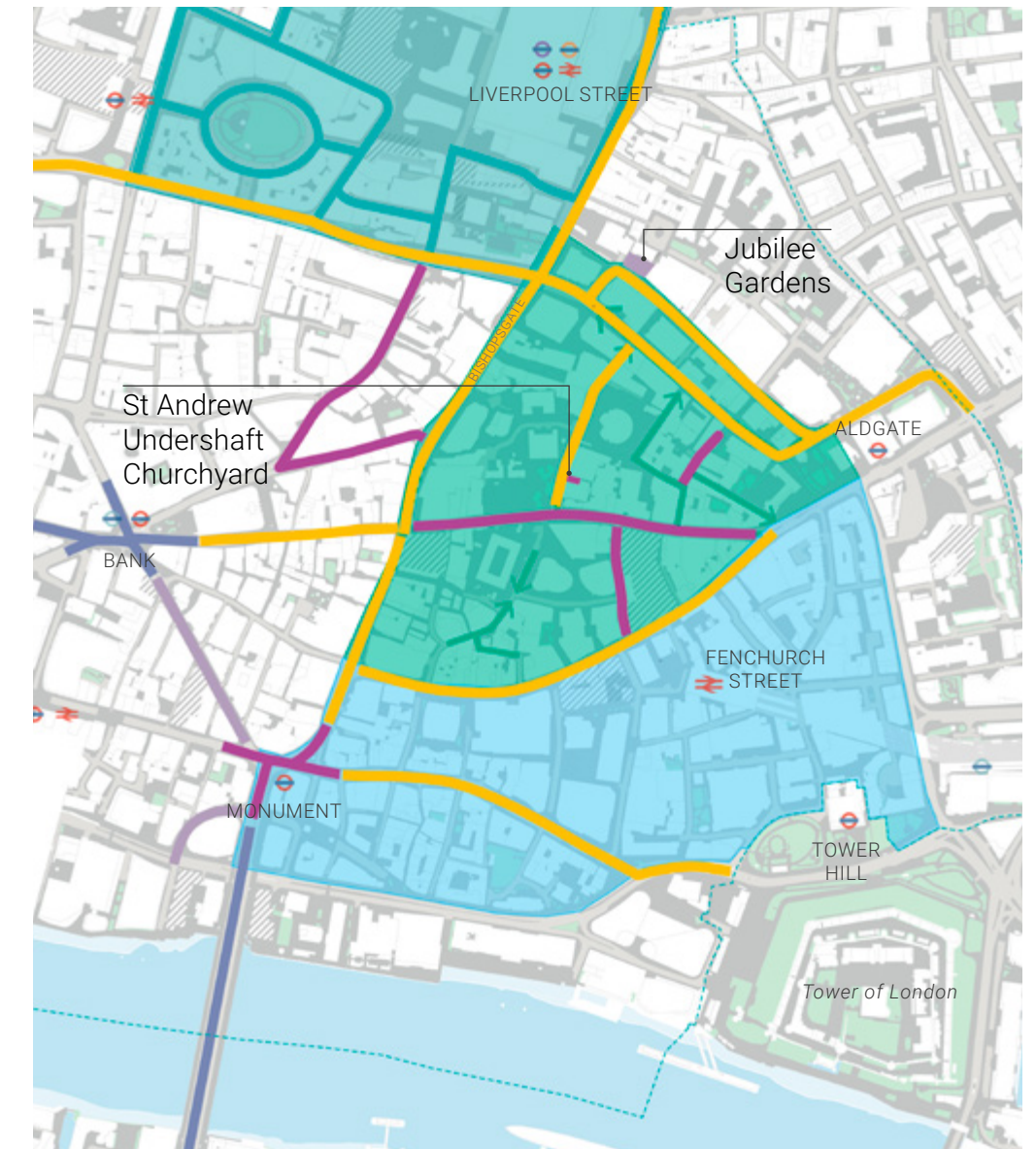


fig. 2 Public realm in the development pipeline

- Future projects
 - Design stage / Consultation
 - In construction
 - Recently implemented
- Healthy Streets Plans:**
- Liverpool Street
 - City Cluster
 - Fenchurch Street



Roman London

- + First settled around 50 AD following the Claudian invasion, and destroyed shortly after by a domestic rebellion.
- + The centre of Roman London lay within the EC BID area.



Seventeenth-century London and the Great Fire of London

- + Political and religious upheaval in England served to solidify the significance of the City as the financial base of the country.
- + Great Fire of London in 1666, destroyed about five-sixths of the city within the walls and 86 of the 106 parish churches in the area. Various plans emerged for an idealistic rebuilding of the city, but individual landowners refused to part with their plots, leading to a reconstruction of the pre-Fire city.
- + Medieval alleys, passages and courtyards served as centres for trade and communication, particularly around Lombard Street (e.g. Change Alley). Coffee houses developed into a 'informal infrastructure' for the exchange of letters and price-lists crucial to the operation of domestic finances and international trade.

Medieval London

- + A new settlement was established on the site of the abandoned Roman town at beginning of the seventh century and emerged as a mercantile centre.
- + During the Saxon inhabitation, the routes of the main streets remained determined by the location of the gates in the Roman wall, with minor streets laid out on a modified grid system.
- + Guilds developed as organisations responsible for social and religious functions, and became increasingly tied to specific crafts and trade.
- + Mass expansion of foreign trade, with the foundation of the Company of Merchant Adventurers in the early fifteenth century, the Royal Exchange, Custom House, and new trading companies.
- + Huge population growth and solidification of London's importance on the national stage.

fig. 3 Illustration of the Roman City of London, Froste, 1996

fig. 4 1616, Visscher's View of London



The Victorian and Edwardian City

- + Further improvements to the urban fabric, like glass roofs, which allowed for top-lit workshop and commercial spaces.
- + Images showing the diversity of frontages with awnings projecting into the street highlight the importance of ground floor premises in enlivening the streetscape.
- + During the early twentieth century, the City was identifiable as the financial capital of the Empire.



fig. 5 Bishopsgate at the end of the nineteenth century

fig. 6 The railroads, including Fenchurch Street Station, were part of the transformation of the City into a predominantly commercial area.

Eighteenth-century and Early Nineteenth-century Improvements

- + Street and infrastructure improvements related to desire to improve circulation in the city.
- + Improvements were coupled with the construction of monumental buildings, including new livery company and financial buildings.
- + Transformation in the character of the city 'from a residential area with specialist commercial and financial functions to a financial and commercial enclave with a dwindling residue of inhabitants.'
- + This process was facilitated by the arrival of railways and the underground.
- + Four-fifths of the city's buildings standing in 1855 were rebuilt by 1901.



World War II and subsequent redevelopment

- + The City suffered extensive bomb damage during the Second World War.
- + The bombing damaged the historic architecture of the city, with 23 of the 49 surviving City churches and 25 of the 36 livery halls badly damaged or destroyed.
- + Following the end of the war, debate continued as to the best way to reconstruct the City, including the design of individual buildings to consider the public realm more broadly.

Late Twentieth Century

- + Increased connection and collaboration between developers and City planners
- + Changes in financial regulations that instigated shifts in the patterns of office developments. This initially began on the fringes of the City, where the banks could occupy large offices of a type familiar from many American cities.
- + Developments including plazas indicated a new approach to the urban environment, necessitating a sweeping away of historic street patterns to create a hard, open landscape and altering the way in which buildings related to streets.
- + Between 1985 and 1993 it has been estimated that about 50% of the total office volume of the City was rebuilt.



fig. 7 1943, Opportunities and Considerations in the redevelopment of the City of London

fig. 8 1969, Commercial Union Tower, Baistow

fig. 9 1969, Commercial Union Tower, Snoek

Today

- + The street pattern in the City today reflects these centuries of development.
- + Creation of new public realm at the base of new developments, as well as the expansion of the public realm to the roofs and into the lobbies of buildings, will continue to develop this historic network of streets and spaces.

fig. 10 The street pattern in the Eastern City represents all periods of the City's history

- Roman
- Medieval streets
- Medieval spaces
- 19th - early 20th centuries
- Postwar - contemporary



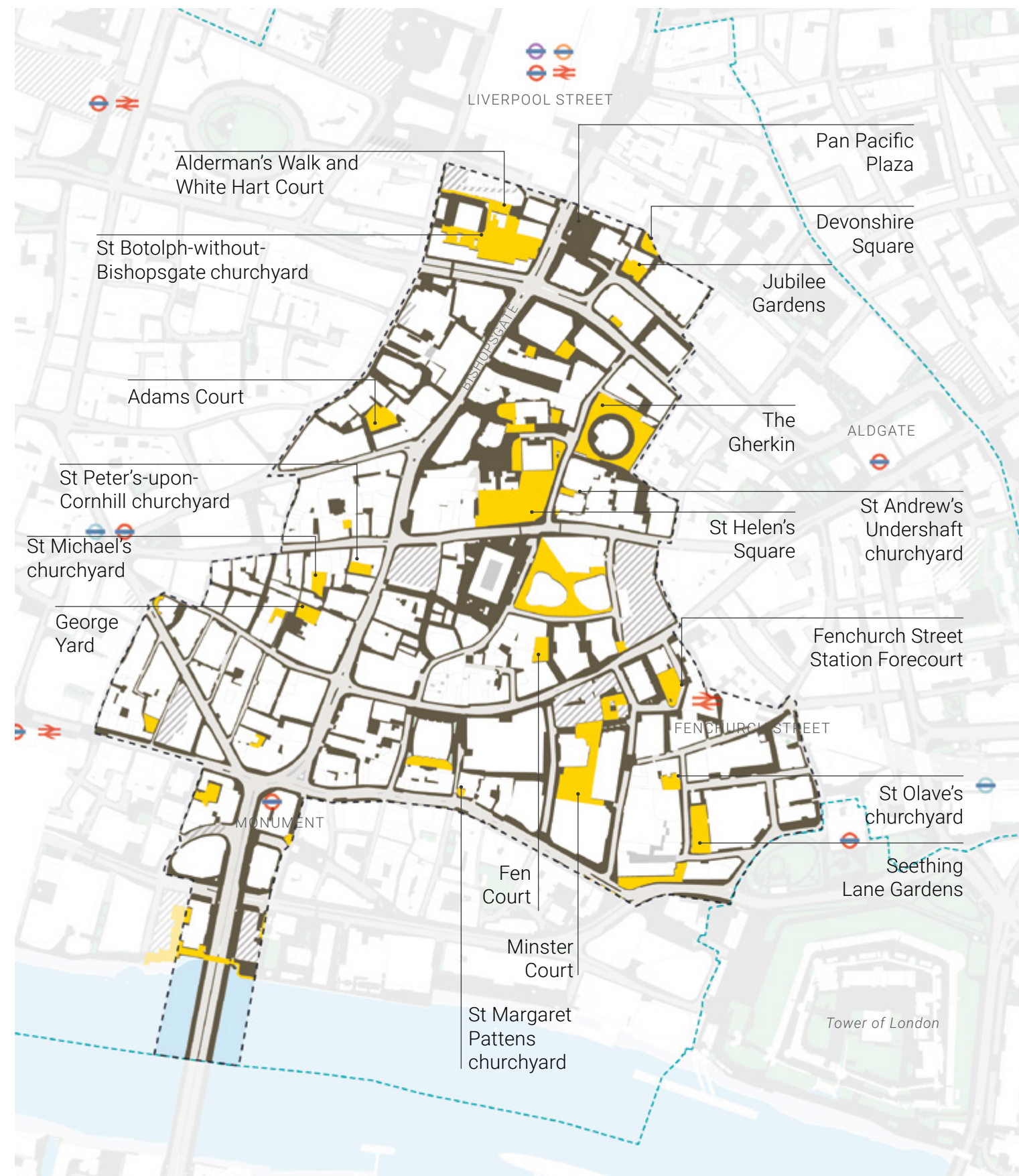


fig. 11 The public realm network across the EC BID area

- Other public realm (pavements, general open spaces)
- City of London defined open spaces

Existing public realm programmes sponsored and implemented by the BID

The BID has already supported a number of programmes, projects, and initiatives that aim to improve the public realm and sense of place across the Eastern City. To date, the BID has worked collaboratively with the City to install clusters of planters and seating in key locations in public highway, and to review dockless bike parking across the City, including particular problem hotspots. The City and the BID are currently in discussions to appoint a gardener who will support the Corporation's City Gardens team and lead periodic tours of the area's hidden green spaces.

The BID has sponsored numerous studies and surveys that help support the case for an improved public realm. These have included a consumer segmentation study advocating for more diverse retail offer, a consolidated freight, servicing and delivery (FSD) study to assess feasibility and adoptability of consolidating servicing in the BID area, and an area-wide study for potential greening projects, which will be implemented across the area in the coming months.

To support efforts to make the City safer and more welcoming, the BID has sponsored a wintertime night safety hub as well as women's safety initiatives. The BID sponsors events for young workers, aiming to welcome a new generation of people to the Eastern City as well as playful installations that bring children and families to the area. The BID's support for the London Festival of Architecture, including a podcasting booth and an urban playground, similarly invites cultural uses and new visitors to the City, and have proven useful as pilots for testing new concepts for activating public spaces. A recent project with Brookfield opened up an indoor lobby space to cultural events and, at Old Broad Street, the BID supported a temporary ground floor use and affordable workspace for creative businesses, in collaboration with Hive Curates and LandSec. As a partner of Sculpture in the City, the BID is committed to bringing art and culture to the public realm across the area.

A new approach

This Vision seeks to align a diverse set of priorities from a range of stakeholders, from workers, to neighbours and visitors, to the City of London Corporation, to the development community and property owners. The BID will champion this partnership approach to improving the public realm, embracing the different ways each stakeholder can contribute to the process.



About the process

Methodology

In December 2023 the EC BID appointed independent research and urban design practice Publica to deliver a long-term vision for the streets and public spaces around the Eastern City area. To ensure a holistic approach and understanding of the public realm, Publica has collaborated with transport consultants NRP, heritage specialists Donald Insall Associates, and public engagement experts Christina Norton and Jessica Cargill-Thompson.

The Publica team spent over 100 hours on site, studying the streets, alleyways, courtyards, plazas, and gardens, to understand how people use these spaces. The team recorded public realm assets, trees, planting, and benches, as well as ground floor vacancy and lobby space. These spatial surveys are included in the separate Survey document delivered as part of this Vision.

We closely read the City Cluster Vision, published in 2019 by the City of London Corporation, and the Asset Audit completed by Publica for the Eastern City Partnership in 2022. These formed the starting point for this Public Realm Vision.

Donald Insall Associates compiled a baseline report of the area's history and important remaining heritage features, using archival imagery, maps, and business records. NRP, the transport consultants, compiled a baseline assessment of the opportunities and constraints of traffic and transport in the area. Both of these reports are included in full in the Survey, and key points have been woven throughout this Vision.

Engagement

Between February and March 2024, we spoke to a diverse range of people, from office workers to students, visitors, residents, families with young children, food-stall vendors, librarians, lunchtime runners, and cyclists. We had conversations with people of different ages, backgrounds, mobility needs, and with priorities and aspirations for the area. People left 100+ comments on post-it notes and comment cards during our on-street pop-up sessions, and we had more than 100 people respond to our online survey.

The team have been working closely with the City of London Corporation to ensure the Public Realm Vision aligns with and supports its policies, current strategies and draft City Plan. We have had formal briefings with Ward members about the project to gain insight into their priorities. We are also working with other statutory bodies, including TfL, to ensure that the Vision supplements and adds value to the work already underway. Conversations with property owners and major stakeholders have also informed our proposals.



fig. 12 Monument, Wednesday 28th February

fig. 13 Artizan Street Library, Thursday 7th March



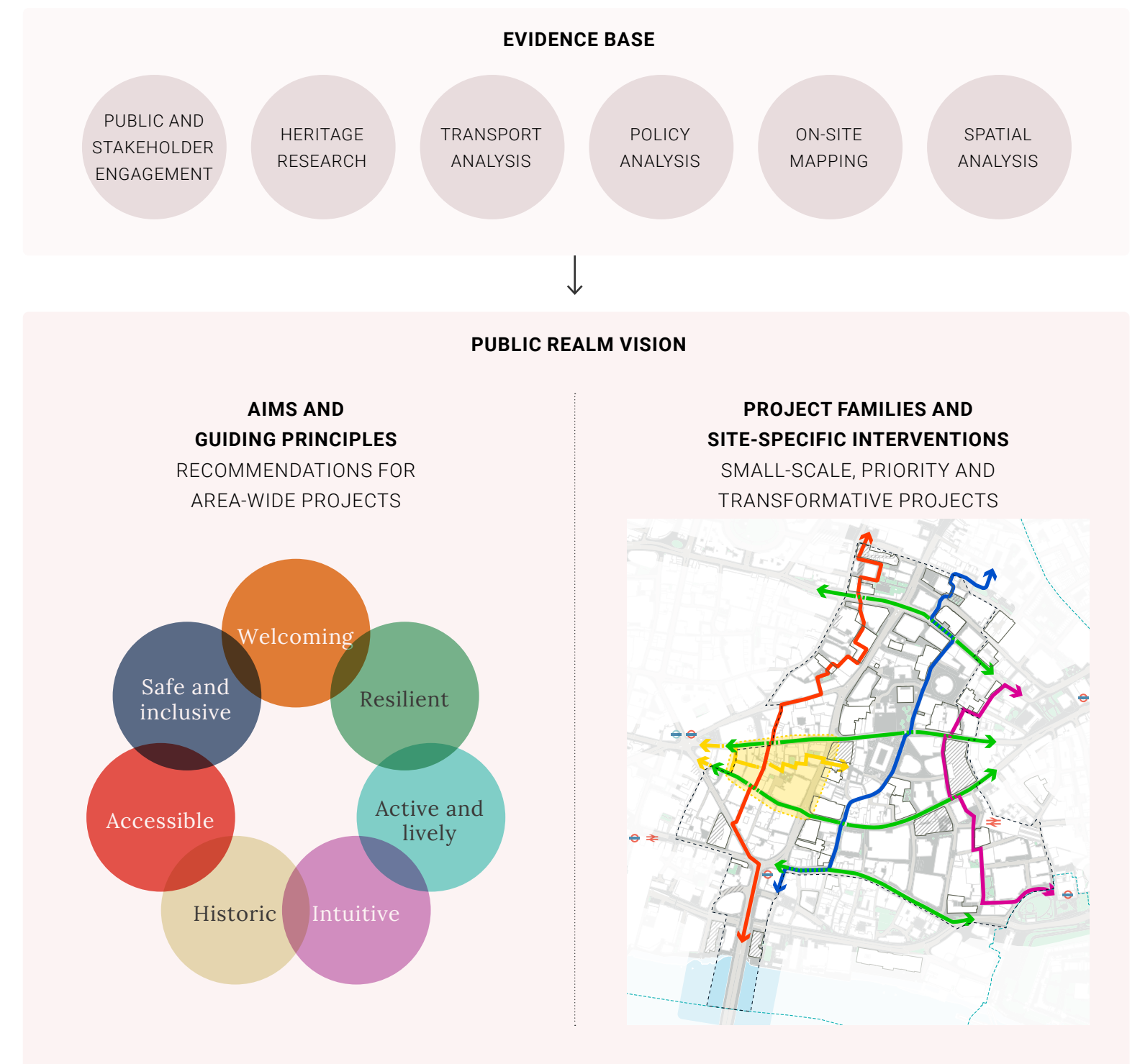
How to use this document

Structure

The Public Realm Vision is divided into two sections:

The first, "Aims and guiding principles," outlines key findings from the research processes and engagement, principles to guide long-term strategy and recommendations for next steps, around which the BID and the area's many stakeholders can coalesce. (The full evidence base that emerged from the survey and engagement process are included in the Survey appendix to this document.)

The second section, "Project families and site-specific interventions", outlines opportunities for public realm improvements across the Eastern City area in the context of current projects and upcoming developments. Suggested improvements align with the guiding principles outlined in the first chapter, and improve the public realm of this area for everyone. Specific projects range from large-scale transformative projects, medium-term priority projects, and smaller-scale interventions that can improve the public realm in the shorter term.



Aims and guiding principles

The seven aims and the supporting thirty-eight principles have been developed to help guide both the projects the BID will take on, as well as the conversations with stakeholders as larger-scale projects are explored.

We envision a future public realm of the Eastern City that is defined by:

Welcoming spaces

Design publicly-accessible spaces that are generous, inviting, and accommodating of the needs of those who work and spend time in, live near, and pass through the area.

Resilient trees and planting

Embrace planting as a key way to address net zero carbon goals and climate adaptation, and ensure the area is as green as it can be through protection of existing planting, innovative techniques that are suited to the area's unique microclimate conditions.

Lively uses and activity

Create a lively place, harnessing the energy of commerce and trade that characterised the area in the past and celebrating the area's cultural and recreational destinations.

Intuitive navigation

Make it easier to find your way around the area, while also making it a rewarding place for wandering and discovery.

Historic character and lighting

Celebrate, reveal, highlight and improve the setting of the heritage features across the area.

An accessible environment for all

Establish environments where all modes of movement can coexist – with priority for walking, wheeling and cycling to contribute to climate and health goals.

A safe and inclusive neighbourhood

Promote streets and spaces where all users feel safe and share in a sense of belonging.



Welcoming.
Resilient.
Active and lively.
Intuitive.
Historic.
Accessible.
Safe and inclusive.



Welcoming spaces

Design welcoming publicly-accessible spaces that are generous, inviting, and accommodating of the needs of those who work and spend time in, live near, and pass through the area.

Guiding principles

1 Outdoor spaces	Celebrate growth while preserving spaces for the public at ground floor.
2 Indoor spaces	Encourage the creation of publicly-accessible indoor or covered spaces at the ground floor of new developments. Work with the City, property owners and buildings operators to transform existing and proposed lobbies into more welcoming internal spaces.
3 Spaces at height	Ensure destinations at height have amenity, are multifunctional, and access to them is clear, legible, and inviting.
4 Dwell space	Create spaces for dwelling at street level, with planting, public art, in addition to cut-throughs that improve permeability and movement.
5 Materiality	Soften the environment with warmer, natural materials in the public realm, and favour low-carbon and circular approaches to materials.
6 Ownership	Acknowledge private ownership to actively invite public use, working together with property owners

Recommendations for area-wide projects

- + Work with the City and property owners to implement light-touch material interventions to the seating across the area
- + Work with the City to establish guidelines for amenity spaces at height
- + Explore concept of a logo to increase the feeling of welcomeness in spaces that are open to the public
- + Map the three-dimensional ground floor experience
- + Develop a ground floor use strategy (including lobbies)
- + Map emerging community amenities and curation opportunities
- + Work with property owners and building managers to test new concepts for their spaces with events, installations and programming, to inform guidelines developed with the City

Key findings from engagement

- + We heard that people value the peaceful and quiet places and desire seats that are comfortable.
- + "There aren't enough places to sit. Everywhere there is to sit is always taken."
- + "None [of the atriums] are open, and none are welcoming, if some of them are, they don't make it clear that you can sit inside and certainly staff want to discourage you from entering."
- + Most people are unaware that some atriums are public.
- + Public space is generally used for lunch, socialising, those seeking peace and others wandering.
- + People would like to see "pedestrianisation of some streets or at least shared surfaces."
- + There is a lack of public space for major gatherings.
- + "Less priority for tall glass towers with no activity at the ground floor, with massive lobbies with nothing."

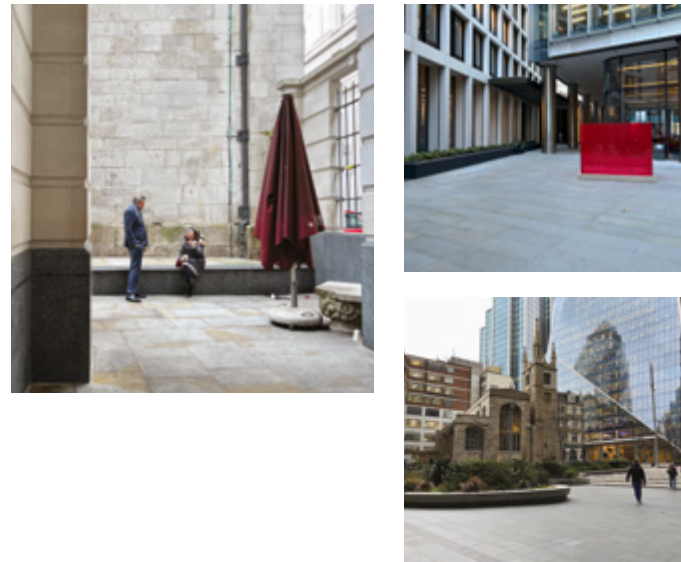
Key findings from research

- + The public realm network is distinguished by contrasting scales and uses, from medieval passageways and low-traffic lanes to contemporary office plazas.
- + The public realm network can be divided into spaces for movement and spaces for dwelling: spaces for movement (e.g. pavements, pedestrian-friendly streets) make up 16% of the BID area, while spaces for respite (e.g. courtyards and churchyards) make up 7% of the area.
- + Office lobbies cover more than 4,000 square metres of ground floor space.
- + Some office lobbies are ostensibly public, but a lack of signage and the presence of security guards do not create inviting spaces. Lobbies with cafés feel more welcoming.
- + The four publicly-accessible spaces at height in the area have differing levels and quality of amenity, and don't function as incidental public space.
- + The area's public spaces and benches use hard and dark materials, like dark granite, which contribute to the harsh environment.



1 Outdoor spaces

Celebrate growth while preserving space at ground floor.



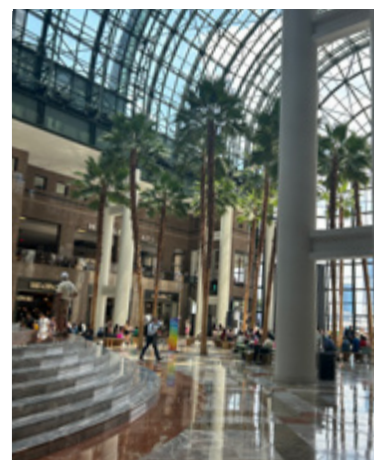
Upcoming developments will put increased pressure on the public realm. In the face of this growth, and the new people it will welcome to the area, it is important to ensure that the ground level across the BID area remains as open to the public as possible. Public space at street level should be prioritised in all new developments to accommodate new workers.

Loss of public realm space at ground level caused by developments should be actively mitigated.

- fig. 14** Courtyards and alleways are precious pockets of outdoor spaces used for pausing and gathering
- fig. 15** Recent developments have added to the network of yard, alleyways and larger open spaces that can be used for both movement and dwelling, if sheltered from environmental conditions
- fig. 16** Open spaces, especially those with planting, are precious in the EC BID area

2 Indoor spaces

Encourage the creation of publicly-accessible indoor or covered spaces at the ground floor of new developments. Work with the City, property owners and buildings operators to transform existing and proposed lobbies into more welcoming internal spaces.



→
See Evidence Base for map of indoor spaces across BID area

While these spaces fall outside of the typical definition of the public realm being “the space between buildings,” and indeed are nearly always privately owned, they form important parts of how we experience the public realm.

The public realm in this area is incredibly constrained, and will become even more constrained as new developments emerge. Existing office lobbies cover more than 4,000 square metres of ground floor space. Expanding the public realm into existing and proposed office lobbies to create a network of interior public spaces will ease the pressure on the public realm.

Challenging environmental conditions can create intense microclimate and render much of the outdoor spaces unusable. Construction noise adds to the unpleasantness of the environment. In this context, indoor public spaces can allow more people to enjoy public space across the day, encouraging more gatherings and public life. Due to their protection from the elements, these spaces can be active at all times of year, creating a welcoming atmosphere with places to sit, rest, eat, and meet friends.

Publicly-accessible amenities in these indoor spaces could include art, community facilities, cultural events, public toilets, cafés, and seating.

- fig. 17** The Winter Garden in New York’s Financial District. Indoor public spaces are not without their challenges: without clear signage and an active relationship to the street these spaces can flounder. Walls and windows that allow for adequate light and visibility between inside and outside, as well as planting and seating are critical to ensuring the success of indoor spaces.

3 Spaces at height

Ensure destinations at height have amenity, are multifunctional, and access to them is clear, legible, and inviting.



→
See Evidence Base for map of existing and forthcoming public spaces at height across BID area

There are two viewing galleries and two rooftop gardens within the BID area, and at least three more projected in the development pipeline. The success of the planning system in securing such spaces as a public benefit should be celebrated, but their limitations should also be clearly articulated.

The provision of public benefit, including publicly-accessible space, should be fundamentally based on need. What other types of spaces can contribute to a sense of place in this area of the City? How do these spaces relate to culture and civic uses? A holistic view of how a development can meet the specific needs and contribute actively to the public life of a place should be the foundation of any evolving policy.

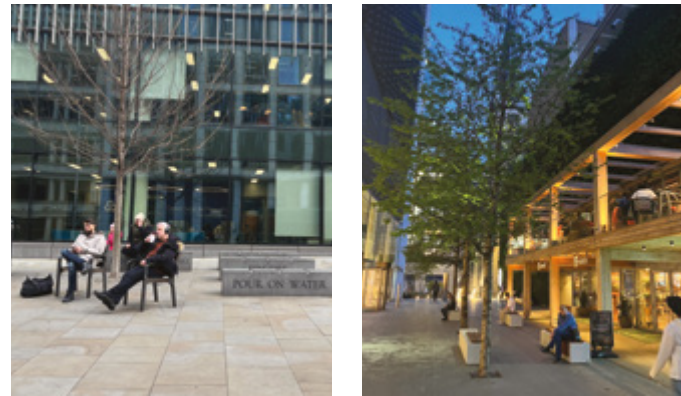
Incidental use is a critical consideration when looking at elevated spaces. While the four existing public viewing galleries and roof gardens across the area are accessible to the general public (and form a successful consolidated tourist attraction) they are not necessarily spaces that are practical to visit for incidental, everyday activities, such as resting before getting on a bus or eating a sandwich at lunchtime. These incidental activities are the backbone of public life and a ‘sense of place.’

To ensure that these spaces act not only as tourist destinations, but also as meaningful public spaces, they need proper amounts of seating, activation, signage, and (where applicable) planting. The most successful examples of elevated public spaces have amenities like seating, informational and engaging interpretive signage, cafés, and shops, as well as signage at ground level that clearly describes the space above as publicly-accessible and actively welcomes members of the public.

- fig. 18** The Garden@120 is visited by both tourists and local workers, who make use of the ample seating and relative ease of access to have lunch outside in temperate weather.
- fig. 19** Horizon 22 provides stunning views over Central London but lacks amenity to welcome all visitors and invite people spend time in the space.

4 Dwell space

Create spaces for spending time at street level, with relevant amenities including seating, planting, public art, in addition to cut-throughs that improve permeability and movement.



→
See Evidence Base for survey of seating

Current City policy (Policy DE4) highlights the need for alleyways and routes within development ownership boundaries, which help reduce congestion on crowded pavements along main routes, and echo the medieval scale of lanes and alleys that define the historic street pattern of the City. This emphasis on encouraging private development to create spaces for movement stands in contrast to the post-war modernist approach to office development, which saw the creation of plazas at the foot of office buildings. While these plazas often turned out to be windswept and vacant, they did preserve substantial areas for dwelling at ground level. These spaces don't necessarily decongest busy footways, but they provide critical amenity for office workers and visitors to spend time outside and create a sense of public life in the area.

With more workers and visitors coming to the area soon, developments should create spaces for people to pass through, but also to spend time in. These spaces must have adequate seating, sunlight, and protection from wind to be functional for incidental use by all members of the public.

fig. 23 Large granite benches and fixed single seats at the Monument allow people to spend time in the space.

fig. 24 The seating along the southern passage of the Sky Garden has ample comfortable seating and is a pleasant place to spend time outside during day and night.

5 Materiality

Soften the environment with warmer, natural materials for public realm amenity, and favour low-carbon and circular approaches to materials.



The hard surfaces of the public realm of the BID area do not create inviting places to sit and spend time. This is particularly true of the granite benches in the area that are cold in the winter and hot in the summer; do not dry quickly after rain, and therefore can't be used at lunchtime after a rainy morning. As recommended in CoL Public Realm Toolkit, timber benches, in addition to solving these issues, also address the overwhelming coldness of the material palette in the area.

The materials for new public realm amenity across the Eastern City should be low-carbon, favouring approaches that reuse and repurpose existing materials, or use materials sourced locally that have a low-carbon supply chain.

fig. 20 Timber slats and armrests integrated to stone blocs at the Strand, Aldwych

fig. 21 Some public seating across the area is hardly used in the winter months given its cold material palette

fig. 22 Timber seating with backrests at the Sky Garden

6 Ownership

Acknowledge private ownership to actively invite public use, working together with property owners.



Most of the publicly-accessible spaces (especially spaces for dwelling) in this part of the City are privately owned, falling within individual land ownership boundaries rather than within the control of the local authority. Some signs help to identify private ownership, often small and with direct, negative language (e.g. "No public right of way"). Overall, there is a lack of clarity about what is private and what is allowed. This potential confusion impacts the public life of a place. If someone is not sure about what they can and can't do, and whether or not they're welcome in a given space, they will likely choose to spend time elsewhere, where the welcome mat is clearly laid out.

Developers and landowners could be encouraged to signpost their private spaces for public use with a recognisable/ bespoke logo to actively invite people in. Private ownership that is communicated passively, through security guards, bollards, ownership studs, or a change of material, can create a barrier to the public use of a space. Clear communication plays a key role in public spaces.

fig. 25 New York City's system of "privately owned public spaces" (POPS) is a prime example of how clearly communicating a place's private ownership can act as an active invitation for people to use the space.

fig. 26 The POPS system, introduced in 1961 through a revision to the city's planning policy that incentivised the creation of public spaces at the foot of tall office towers. After 13 years, more than 500 of these space were created, and a review panel recommended certain changes to their design, including the creation of a logo and signage programme that would clearly label these spaces as publicly-accessible. The resulting "broccoli" logo, which was replaced in 2019, acts as a clear signal that spaces—even indoor ones, or ones that can feel relatively private—are intended for public use.

fig. 28 'No public right of way' sign in the EC area doesn't contribute to creating a welcoming environment.

fig. 29 The contrast between the stone and brick of heritage facades with the glass and steel of contemporary skyscrapers adds dynamism and complexity to the material palette of this part of the City.



Resilient trees and planting

Embrace planting as a key way to address net zero carbon goals and climate adaptation, and ensure the area is as green as it can be through protection of existing planting, innovative techniques that are suited to the area's unique microclimate conditions.

Guiding principles

1 Climate change	Soften the environment with additional planting that uses resilient species that will survive the area's harsh conditions.
2 Diverse planting	Learn from the successes of the diverse planting types of churchyard gardens and encourage biodiverse planting palettes in public realm schemes.
3 Lighting to planting	Keep lighting to planting minimal to not interfere with insect habitat and thereby improve biodiversity of the area.
4 Vertical planting	Encourage green walls and vertical planting only in places where they will get adequate levels of sunlight.
5 Wind and shade	Design for challenging environmental conditions.

Recommendations for area-wide projects

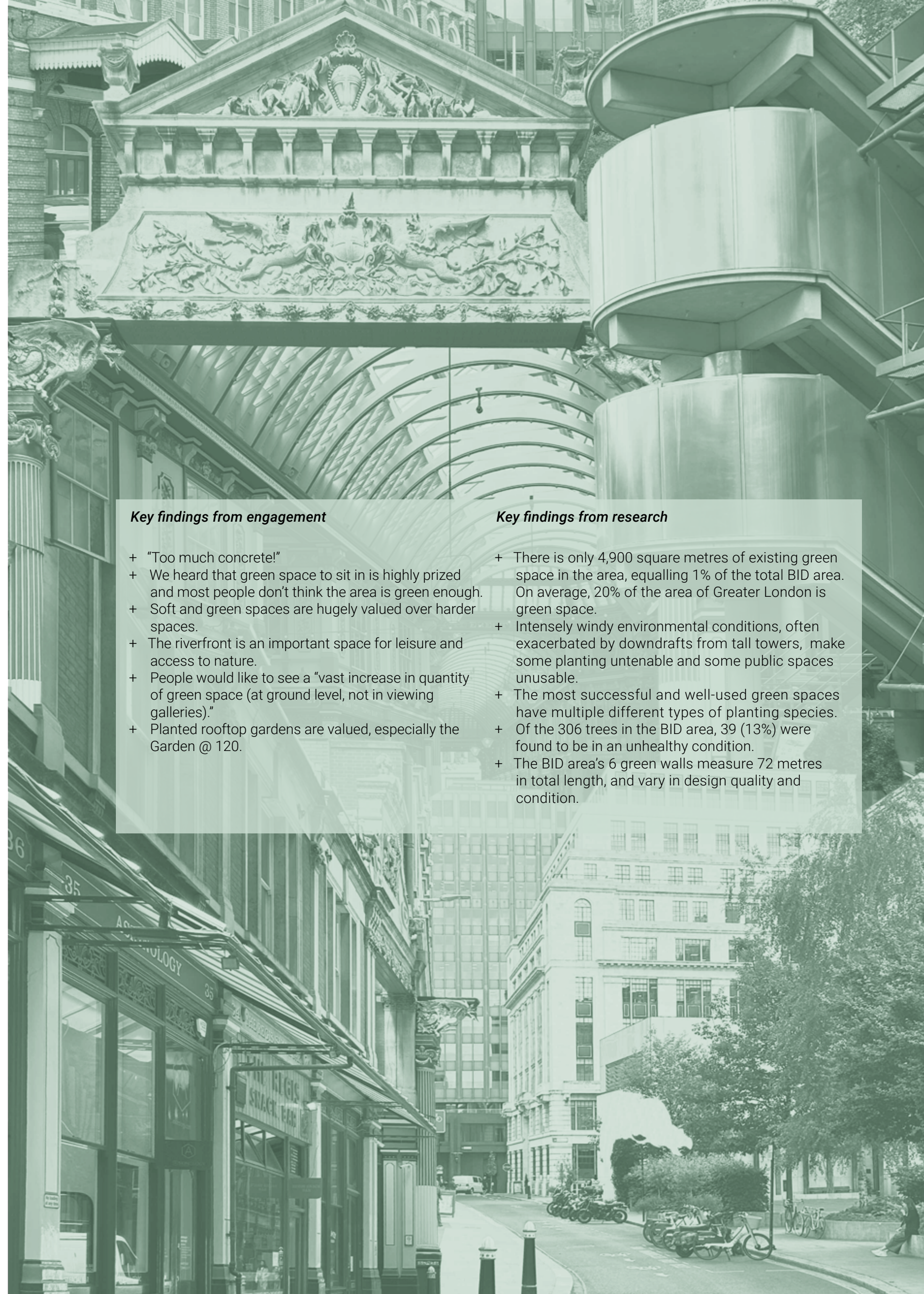
- + In partnership with the City and property owners, seek opportunities for additional planting across the BID area
- + Signpost to the treasured and hidden gardens by producing a 'Green Spaces' map to support quarterly tours led by our EC BID gardener

Key findings from engagement

- + "Too much concrete!"
- + We heard that green space to sit in is highly prized and most people don't think the area is green enough.
- + Soft and green spaces are hugely valued over harder spaces.
- + The riverfront is an important space for leisure and access to nature.
- + People would like to see a "vast increase in quantity of green space (at ground level, not in viewing galleries)."
- + Planted rooftop gardens are valued, especially the Garden @ 120.

Key findings from research

- + There is only 4,900 square metres of existing green space in the area, equalling 1% of the total BID area. On average, 20% of the area of Greater London is green space.
- + Intensely windy environmental conditions, often exacerbated by downdrafts from tall towers, make some planting untenable and some public spaces unusable.
- + The most successful and well-used green spaces have multiple different types of planting species.
- + Of the 306 trees in the BID area, 39 (13%) were found to be in an unhealthy condition.
- + The BID area's 6 green walls measure 72 metres in total length, and vary in design quality and condition.



1 Climate change

Soften the environment with additional planting that uses resilient species and nature-based solutions to the risks of climate change.



fig. 32 Planting beds at grade act as buffer zones to vehicular traffic, providing areas for seating while helping with rainwater run-off.

Climate change will bring heightened risk for drought, fire, and stormwater flooding to the City of London, and through the use of planting, the public realm can be a critical stage where climate adaptation and mitigation takes place.

Urban tree canopies reduce heat by providing shade, and sequester carbon in the air. Nature-based solutions to flooding, like sustainable drainage systems (SuDS), address higher flood risk while also increasing biodiversity and adding beauty to the streetscape. Incorporating appropriate and resilient plants ensure that more of this planting thrives in the unique environment of the BID area. The City Gardens team has guidelines that help outline plant species that can survive the intense environmental conditions, and these should be followed in new developments, and the Corporation's Climate Action Strategy similarly highlights the role of planting in mitigation and adaptation.

2 Diverse planting

Learn from the successes of the diverse planting types of churchyard gardens and encourage biodiverse planting palettes in public realm schemes.

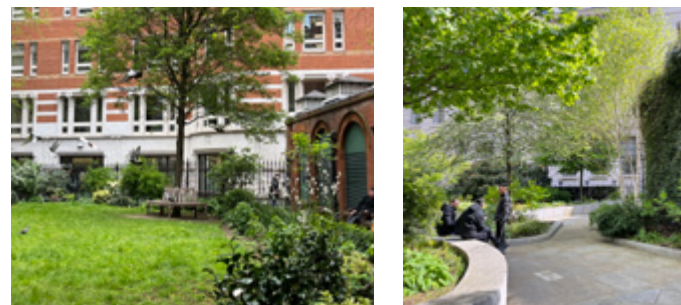


fig. 33 St Michael's Churchyard provides a mix of seasonal planting with various interests across the seasons

fig. 34 Seething Lane Garden offers a variety of planting types and species

Churchyards have many different types of plants, from ornamental trees to shade trees to vines, hedges, grasses, and flowers. These treasured green spaces can act as a blueprint for more diverse planting in other places around the BID area. Single-species planting schemes should be avoided.

3 Lighting to planting

Keep lighting to planting minimal to not interfere with insect habitat and thereby improve biodiversity of the area.

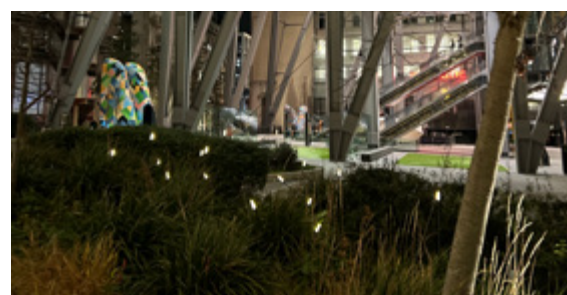


fig. 35 St Helen's Square includes small lighting elements within the planting which is small-scale enough to respect biodiversity and habitat-creation.

Lighting can elevate the aesthetics of trees and planting, and ensure that public spaces are pleasant and comfortable to spend time in after the sun goes down. However, lighting also affects the ability of plants to support insect habitat and other critical functions that support biodiversity. Lighting should be done sensitively, always in accordance with biodiversity guidelines.

4 Vertical planting

Encourage green walls and vertical planting only in places where they will get adequate levels of sunlight.

Green walls and vertical planting are innovative methods for adding greenery to a constrained and dense environment. However, lack of sunlight can lead to unhealthy green walls, which are eyesores of bland sedum. Moreover, green walls are incredibly water-intensive, which are untenable in the face of increased drought risk. Green walls are also water-intensive, rendering them unsustainable in a future with more droughts. Therefore, green walls should be used sparingly, and only where they have a high chance of survival.

Vertical planting and other innovative approaches could also be used on bus stops, pillars, wind breaks, and other built structures.

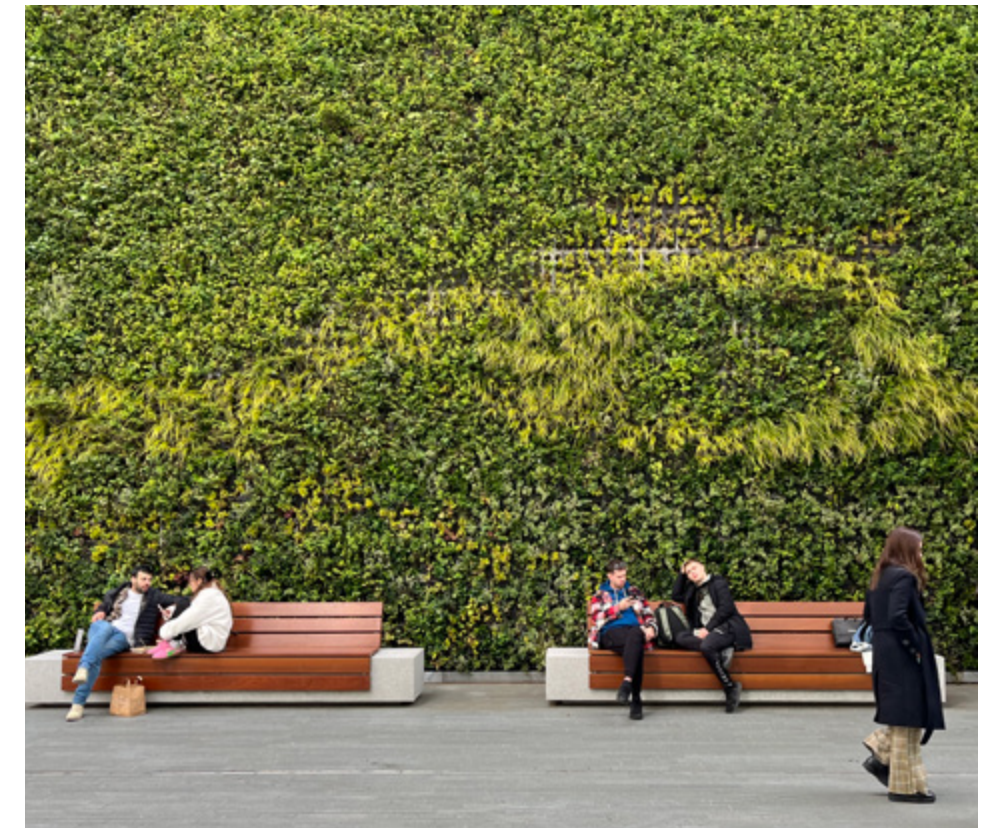


fig. 36 Thriving vertical planting at Sky Garden

fig. 37 Temporary living wall on construction hoarding in King's Cross

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See Evidence Base for survey of planting

5 Wind and shade

Design for challenging environmental conditions.



Public spaces in the Eastern City are characterised in part by the shade and wind created by tall towers, which renders much of the outdoor space unusable. These conditions will only get worse with climate change creating more intense weather and new towers creating new downdrafts. In this context, landscape designers and gardeners can't rely on a standard planting palette. New and innovative approaches need to be leveraged.

→
See Evidence Base for sun studies

fig. 38 Windy environmental conditions make some public realm unusable, both for people and trees.

fig. 39 There are concentrated patches of trees whose health is affected by the wind.

Lively uses and activity

Create a lively place, harnessing the energy of commerce and trade that characterised the area in the past and celebrating the area’s cultural and recreational destinations.

Guiding principles

1 Markets	Build on existing success and embrace streetfood markets as a way to add activity to the street at lunchtime and outside of working hours.
2 Visual Interest	Work with the City to allow for posters, banners, and other graphics on the streets in specific areas, avoiding additional street clutter by using walls and construction hoarding.
3 Meanwhile uses	Promote meanwhile and temporary cultural uses in new developments and vacant sites.
4 Events	Support programmes of festivals and events and equip larger open spaces with required infrastructure.
5 Weekend opening hours	Encourage businesses to remain open over the weekend.
6 Construction hoardings	Embrace construction hoardings as a way to add colour and texture to the streetscape.

Recommendations for area-wide projects

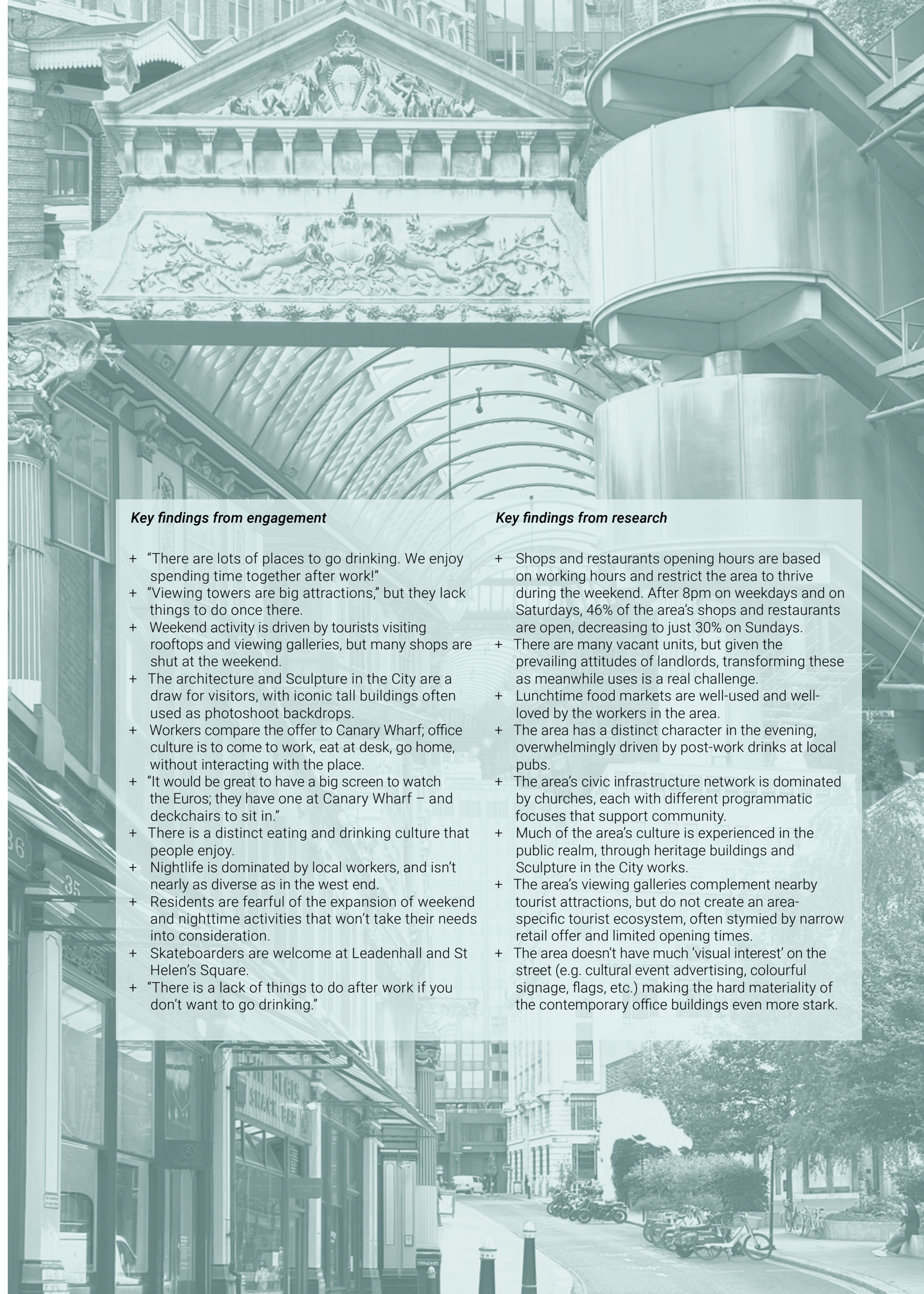
- + Lobby for relaxed market policy
- + Fund the graphic design for banners and posters showcasing cultural events
- + Alleyways arts programme
- + Construction hoarding arts programme
- + Sponsor a business case for weekend opening hours
- + Replicate successful models for meanwhile use across other locations in the BID area
- + Construction hoarding graphic design toolkit

Key findings from engagement

- + “There are lots of places to go drinking. We enjoy spending time together after work!”
- + “Viewing towers are big attractions,” but they lack things to do once there.
- + Weekend activity is driven by tourists visiting rooftops and viewing galleries, but many shops are shut at the weekend.
- + The architecture and Sculpture in the City are a draw for visitors, with iconic tall buildings often used as photoshoot backdrops.
- + Workers compare the offer to Canary Wharf; office culture is to come to work, eat at desk, go home, without interacting with the place.
- + “It would be great to have a big screen to watch the Euros; they have one at Canary Wharf – and deckchairs to sit in.”
- + There is a distinct eating and drinking culture that people enjoy.
- + Nightlife is dominated by local workers, and isn’t nearly as diverse as in the west end.
- + Residents are fearful of the expansion of weekend and nighttime activities that won’t take their needs into consideration.
- + Skateboarders are welcome at Leadenhall and St Helen’s Square.
- + “There is a lack of things to do after work if you don’t want to go drinking.”

Key findings from research

- + Shops and restaurants opening hours are based on working hours and restrict the area to thrive during the weekend. After 8pm on weekdays and on Saturdays, 46% of the area’s shops and restaurants are open, decreasing to just 30% on Sundays.
- + There are many vacant units, but given the prevailing attitudes of landlords, transforming these as meanwhile uses is a real challenge.
- + Lunchtime food markets are well-used and well-loved by the workers in the area.
- + The area has a distinct character in the evening, overwhelmingly driven by post-work drinks at local pubs.
- + The area’s civic infrastructure network is dominated by churches, each with different programmatic focuses that support community.
- + Much of the area’s culture is experienced in the public realm, through heritage buildings and Sculpture in the City works.
- + The area’s viewing galleries complement nearby tourist attractions, but do not create an area-specific tourist ecosystem, often stymied by narrow retail offer and limited opening times.
- + The area doesn’t have much ‘visual interest’ on the street (e.g. cultural event advertising, colourful signage, flags, etc.) making the hard materiality of the contemporary office buildings even more stark.



1 Markets

Build on existing success and embrace streetfood markets as a way to add activity to the street at lunchtime and outside of working hours.



→
See Evidence Base for map of market locations

The existing lunchtime food markets at the Gherkin (Thursdays), Monument (Wednesdays), and Fenchurch Street Station forecourt (Tuesday through Friday) are all beloved by City workers, often cited as a nice reason to get out of the office and a distinguishing element of working in the City. Similar food markets are proposed in a handful of the current development proposals, as a way to activate the ground floor publicly-accessible spaces. Through conversations with the City, the BID can help ensure that the proposed markets are supported by policymakers as proven ways to add life to the streets. Food markets could particularly impactful after working hours, as events in and of themselves.

fig. 40 Lunchtime food markets taking place across the area are popular among workers and support activation of the public realm.

2 Visual Interest

Work with the City to allow for posters, banners, and other graphics on the streets in specific areas, avoiding additional street clutter by using walls and construction hoarding.



The hardness of the environment of the BID area is partially caused by lack of greenery and the materiality of the built environment. It is also caused by a near-total lack of visual interest on the street. Visual interest includes any element of the built fabric that breaks the glass, steel, and granite of area's architecture. Heritage features and buildings, like the small historic signs on Lombard Street and Cornhill, are a type of visual interest, as are the historic signs in the alleyways near the Jamaica Wine Bar. These small-scale graphic elements of the built environment are treats for the eye, and add grain and texture to an otherwise monotonous cityscape.

The irony of the City's restrictive advertising and printed ephemera policies is that the exact heritage condition that it seeks to protect was itself defined by much more visual interest on the streets. Looking at old photos of the alleyways and even of Bishopsgate, the spaces across the BID area were a riot of colour, activity, and commerce. A small dose of colour and character provided through printed material would go a long way in establishing a sense of place in the area. Done correctly, this can only add to the heritage setting, rather than threaten it.

Like public art, visual ephemera also allows for improved navigation and telling stories about the area's past.

fig. 41 Blank wall used for promoting cultural events in La Defense, Paris.

fig. 42 BID-sponsored banners in the streets of New York, add colour to the predominantly brick- and stone-toned built fabric.

fig. 43 The BID sponsored vinyls on Fountain House, which added colour and vitality to the streetscape.



fig. 44 Bishopsgate in 1890, dressed with awnings

fig. 45 Crutched Friars in 1912, lined with signage as decoration to buildings elevations

fig. 46 Bishopsgate in 1912, dressed with awnings and advertisements

fig. 47 Gracechurch Street in 1870.

3 Meanwhile uses

Promote meanwhile and temporary cultural uses in new developments and vacant sites.



→
See Evidence Base for map of vacant shopfronts

'Meanwhile use' describes a temporary or pop-up programme in a building that is under development.

We understand that, from a landlord's perspective, it is often more reliably profitable in the City to keep a ground floor unit vacant than allowing a short-term or meanwhile use. Whether or not it is actually more cost-effective to keep a vacant unit vacant, the mentality can stymie the feeling of vibrancy in an area, by keeping what could be active frontages bare.

There is a specific opportunity to let vacant or pre-development spaces to cultural or creative uses, such as galleries, theatre groups, or craft workshops, as has been recently done at Broadworks, 99 Bishopsgate, and Theatre Deli. This would bring new uses and users into the area. This strategy is impactful in both the upper and ground floor levels of a building. Visibly occupied, lively, and creatively productive ground floor spaces contribute to a vibrant streetscape.

The BID can help facilitate meanwhile uses through financial support including rates mitigation, and convening stakeholders to make matches between tenants and landlords.

fig. 48 Printmaking event and exhibition opening at Broadworks by Hive Curates, a new cultural hub established as part of a meanwhile strategy for vacant sites across the City

4 Events

Support programmes of festivals and events and equip larger open spaces with required infrastructure.



fig. 49 The BID sponsors a summer events schedule that draw people to the public spaces of the Eastern City.

Outdoor events throughout the year can help add life to public spaces. As with food markets, events do not have to be large scale to have an impact; more impactful than scale is regularity and consistency: our sense of place is tied innately to a sense of time, the rhythm of the year, seasons, sporting events, and other cultural milestones. But for these events to be successful and deliverable, public spaces must include appropriate infrastructure like power hookups and back-of-house and servicing areas. The BID can play a role not only in setting up and hosting a calendar of events, but also in ensuring that new publicly-accessible spaces brought forward through private development and City-led improvements have adequate space and infrastructure to host events.

5 Weekend opening hours

Encourage businesses to remain open over the weekends.



fig. 50 Extended opening hours and diversification of uses. A night-time diversification grant allowed this vintage record store in Sydney to curate a diverse range of evening and night-time events and activities including live music, dance classes, film screenings and workshops with local artists.

→
See Evidence Base for map of opening hours

During our Saturday engagement session, people who had come into the City to visit the roof terraces commented that they were heading to the West End or to Shoreditch in search of food options—there was nothing open in the BID area that they could have visited after seeing the roof terraces. Opening hours and increased footfall and spend is a virtuous cycle, but it must begin with broader opening hours.

Many lessons can be learned from processes that have encouraged later opening hours in the context of supporting the night-time economy. The night-time economy in the Eastern City is relatively strong, but similar approaches to supporting local businesses in taking a risk in expanding opening hours could be leveraged by the BID. The BID could support the development of a business case and research piece to explore the benefits of opening hours, or help provide incentives like discretionary rates relief.

6 Construction hoardings

Embrace construction hoardings as a way to add colour and texture to the streetscape.



fig. 51 Construction hoardings provide a canvas for visual interest across the Eastern City.

Construction hoardings are an endemic part of the City's landscape, and will remain so for the next 10 years at least. These temporary structures provide an ideal canvas for environmental graphics that can contribute more to the fabric of the City than advertising for the building that's being built. Thematic or historic supergraphics, or wayfinding elements, could be added to hoardings to both provide useful navigational information and also add needed texture to the Eastern City.

The BID can work to develop a toolkit for developers to assist in ensuring that these features can actively contribute to the visual landscape of the Eastern City.

Intuitive navigation

Make it easier to find your way around the area, without removing the sense of surprise and delight that comes with wandering and discovering new places.

Guiding principles

1 Legible London	Support the continued implementation of Legible London signs and encourage developments to contribute to this existing system.
2 Wayfinding system	Develop an area-specific wayfinding system that complements the London-wide Legible London system and celebrates the Eastern City's heritage.
3 Repaving	Repave specific streets to encourage use of passages and alleyways.
4 Naming	Encourage new indoor and outdoor public spaces to be named, and work with landowners to name existing unnamed open spaces.

Recommendations for area-wide projects

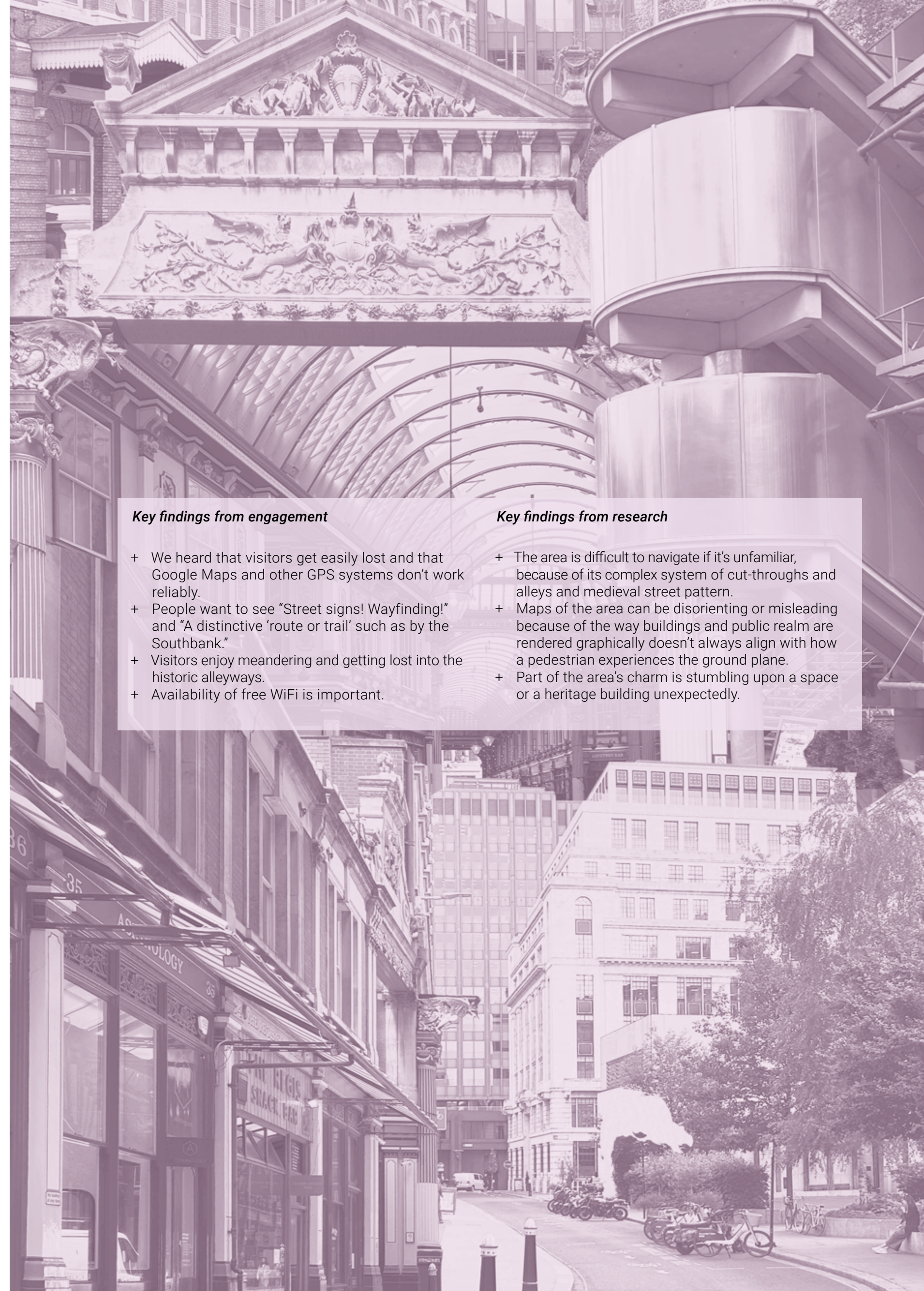
- + Explore a wayfinding programme drawing on heritage and tourist destinations
- + Host a naming competition for unnamed spaces across the BID area

Key findings from engagement

- + We heard that visitors get easily lost and that Google Maps and other GPS systems don't work reliably.
- + People want to see "Street signs! Wayfinding!" and "A distinctive 'route or trail' such as by the Southbank."
- + Visitors enjoy meandering and getting lost into the historic alleyways.
- + Availability of free WiFi is important.

Key findings from research

- + The area is difficult to navigate if it's unfamiliar, because of its complex system of cut-throughs and alleys and medieval street pattern.
- + Maps of the area can be disorienting or misleading because of the way buildings and public realm are rendered graphically doesn't always align with how a pedestrian experiences the ground plane.
- + Part of the area's charm is stumbling upon a space or a heritage building unexpectedly.



1 Legible London

Ensure that the network of Legible London signs is expanded at key locations and encourage developments to contribute to this existing system.



See Evidence Base for map of Legible London signs

One risk of a dynamic development pipeline is that each development may have its own wayfinding programme, with a distinct graphic identity and existing only within its red line. This approach could add visual clutter and confusion to the streetscape. London has a city-wide wayfinding system called Legible London. Managed in part by local authorities and in part by TfL, Legible London is a successful signage programme that has improved navigation throughout the city. Wherever new directional signage is needed in the Eastern City, it should be part of the Legible London system. This will help visitors and locals alike identify stations and directions, because it is a system they are already familiar with. Unless in extreme circumstances, developments should use directional signage that is in line with Legible London standards and contribute to the existing system.

fig. 52 Legible London totems across the area support explicit navigation, especially in areas with poor phone reception due to tall buildings

2 Wayfinding system

Develop an area-specific wayfinding system that complements the London-wide Legible London system and celebrates the Eastern City's heritage.



While Legible London is successful and recognisable for simple A-to-B directions, and should be supported over development-specific wayfinding system, there is an opportunity to develop a distinct wayfinding system for the BID area as a whole.

England has a rich history of innovative wayfinding techniques and systems (of which Legible London is a modern example). One particularly interesting medieval wayfinding technique was the use of "routing" to describe directions between specific cities. Where today, Legible London is about locating oneself and being able to navigate through space, these "scroll maps" are more like a curation of points along a specific journey. Drawing inspiration from this historic source could provide an interesting and engaging way to both add visual interest to the streets and also highlight the area's unique destinations and hidden gems. Alleyway walls and construction hoardings provide canvases for this sort of navigational approach.

Though this system would be explicitly about navigation, it would contribute to a sense of place by adding visual interest to the streets and foregrounding a graphic identity for the area.

fig. 53 A scroll map from *Itinerarium Angliæ: or, a book of roads of England and Wales*, by John Ogilby, 1675

fig. 54 Retained heritage signs reveal the area's historic character and aid in wayfinding even today



3 Repaving

Repave specific streets to encourage use of passages and alleyways.



fig. 55 Repaving of Birchin Lane supports wayfinding and encourages pedestrian movement as an alternative north-south route

People can be encouraged to use certain routes through aesthetics alone: a paved street like Birchin Lane is a more pleasant place to walk than the crowded pavements of Bishopsgate. Certain key streets can be repaved to encourage people to use secondary routes through the BID area: Nicholas Lane and Finch Lane would complete the route begun by the repaving of Birchin Street, and connect King William Street with Threadneedle Street, creating an alternate north-south route between Monument and Old Broad Street. Cullum Street, which lies directly across the street from the Sky Garden, could become an attractive route to Leadenhall Market if repaved. Seething Lane could connect Eastcheap and the Tower of London beyond it to Fenchurch Street Station, Sky Garden, and the Garden@120.

4 Naming

Encourage new indoor and outdoor public spaces to be named, and work with landowners to name existing unnamed open spaces.

Places with names will be more easily identifiable, and therefore better used and more successful. Places with clear names are often easier to navigate to and around. But on a more ephemeral level, a name helps cultivate a sense of place. A collective understanding of place helps create a community. Most City workers know where and what St Helen's Square is, they know what the Cheesegrater, the Walkie Talkie, the Gherkin all are. But while buildings often get formally named, public spaces around them can often go nameless. Without a name, it's difficult to become a place. Names can provide moments to celebrate heritage, too: Change Alley, for example, is named for its proximity to the Royal Exchange, and stands as a reminder of that area's history of commodity trading; Billiter Street references the occupational name for a person who makes bells; the "fen" in Fenchurch Street and Fen Court Garden may reference the area's formerly marshy ground.



fig. 56 The public spaces created at the base of The Scalpel suffer from a lack of identity, partly because it is nameless. Naming such spaces will support creating a sense of place

Historic character and lighting

Celebrate, reveal, highlight and improve the setting of the heritage features across the area.

Guiding principles

- 1 Tangible, natural, and intangible heritage** Celebrate all kinds of heritage throughout the Eastern City, allowing people to view, connect and interpret and find joy in everyday spaces through links to its history.
- 2 Heritage façade lighting** Light heritage façades at key points to help navigation and add to visual interest of the street after dark.
- 3 Alleyway lighting trial** Undertake a lighting trial for the historic alleyways and cut-throughs throughout the area.
- 4 Church spire lighting** Promote the network of churches as important heritage assets, and unveil churches' spires at night through a considered lighting scheme.

Recommendations for area-wide projects

- + Explore a lighting pilot for the historic alleyways
- + Explore a lighting pilot church spires and façades
- + Visual storytelling to bring the history of the area to life

Key findings from engagement

- + "The old passageways and lanes provide authenticity and charm due to their human scale."
- + "It's important not to lose the historic charm of the area."
- + We heard people value the history and heritage of the area such as the historic lanes, courtyards and churches and Leadenhall Market and that these should be celebrated more.
- + Please love learning about the area's rich history.
- + The architecture, old and new, attracts visitors.

Key findings from research

- + The contrast between the historic urban fabric and buildings and the contemporary architecture is the most unique condition about this area.
- + This contrast still has a strong presence on the urban pattern and overall experience of the public realm. Medieval yards and laneways to the west of Gracechurch Street and Bishopsgate have found their contemporary equivalent in the larger privately-owned covered walkways and piazzas to the east.
- + Some heritage façades and pubs act as lit beacons after dark, but many notable features are unlit.
- + Churches across the area are not lit.

→ See Evidence Base for full heritage study



1 Tangible, natural, and intangible heritage

Celebrate all kinds of heritage throughout the Eastern City, allowing people to view, connect and interpret and find joy in everyday spaces through links to its history.



Heritage goes beyond just the buildings and pavements of a place. It is also the ground and rivers that have been built on for centuries, and the stories that this natural and built fabric holds. These types of heritage can be described as tangible heritage in its buildings, streets, monuments, and archaeology, natural heritage in its topography and intangible heritage in local customs, crafts, skills and traditions.

These different types of heritage can be marked through informational elements and activities, like signage and walking tours, both of which are designed to inform people of the past. But these different heritages can also be celebrated through more understated ways that integrate histories into the experience of a place. These later strategies could include the highlighting of heritage features, reintroducing craft and traditions into shopfronts and ground-floor spaces, and bringing back associated signage and street dressings, and planting diverse native plants that recall the area's natural past.

Informational interventions can provide insight into how society, buildings, spaces, practices, and customs have changed over time. More subtle approaches can simply add delight to how people experience the space, even without a formal acknowledgment that a given intervention is done through a heritage lens.

Whether overt or subtle, establishing connections with the past is an important way for people to develop a relationship to a given place.

The heritage in this area of the City is not always joyful, however. This guiding principle should be read in tandem with those under Aim 7.

fig. 57 Informational signage marks buildings and institutions that have since been lost to history.

fig. 58 Heritage patterns, materials, and signage can add texture and delight to an urban scene, even without being formally acknowledged or explained.

2 Heritage façade lighting

Light heritage facades at key points to help navigation and add to visual interest of the street after dark.



There are a number of heritage façades across the BID area that could be lit to enhance the heritage streetscape. The BID can liaise with building owners and managers to develop lighting schemes for these buildings and small-scale heritage features.

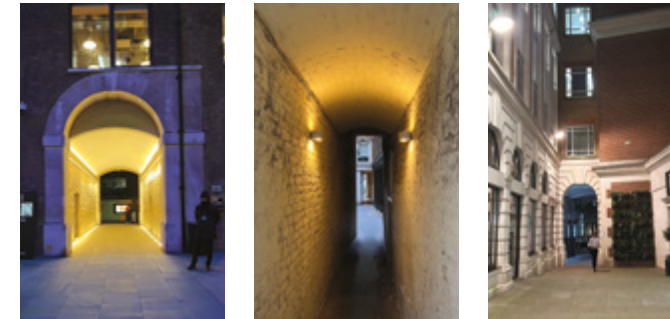
→
See Evidence Base for survey of heritage lighting

fig. 59 On Eastcheap, the heritage buildings along the northern pavement are well-lit, in contrast to the buildings on the southern pavement, which are similarly characterful but remain unlit.

fig. 60 Facades on Lombard Street are well-lit. Discrete lighting could be added to the signage to uncover and reveal part of these heritage features after dark.

3 Alleyway lighting trial

Undertake a lighting trial for the historic alleyways and cut-throughs throughout the area.



The City's Lighting SPD includes many details on the temperatures, lighting levels, and mounting heights of lights across the Square Mile. Due to their small scale, the BID area's alleyways aren't captured in the City-wide in the SPD. These distinctive heritage features could be lit in a consistent way that encourages pedestrians to explore them while feeling welcome and safe.

fig. 61 The network of alleyways across the area is lit unevenly using diverse types of light fittings and mountings. Light fittings pointing up could replace downlighters in these covered spaces to provide a more sensitive approach to lighting

4 Church spire lighting

Promote the network of churches as important heritage assets, and unveil churches' spires at night through a considered lighting scheme.



A famous view of the City of London from the mid sixteenth century, captured from the south bank of the Thames, depicts the many church spires that defined the city's skyline then. Today, it is the skyscrapers that define the skyline of the City, but the spires are nestled within them and could still play a role in the City's identity. Drawing on the example of Bath, which lights its church spires at certain secular and religious days throughout the year, the church spires of the BID area could be lit at certain evenings throughout the year, marking a place-specific calendar of events ranging from Christmas to the marathon to the opening of Sculpture in the City to industry-specific days of importance (e.g. exam days).

fig. 62 The spire of St Margaret Pattens has an excellent aspect down both Plantation Lane (shown) and Eastcheap.



fig. 63 Vischer's View of London from 1616 emphasises churches' spires as landmarks

fig. 64 Today's tall buildings skyline echoes past churches' spires

An accessible environment for all

Establish environments where all modes of movement can coexist – with priority for walking, wheeling and cycling to contribute to climate and health goals.

Guiding principles

- 1 Walking and wheeling** Widen pavements at key locations, introduce new pedestrian crossings, and enhance pedestrian priority on local streets.
- 2 Cycling** Improve provision for safe cycling on streets with high volumes of motor vehicles and enhance permeability on quiet routes. Provide additional cycle parking where possible.
- 3 Public transport** Ensure proposed schemes maintain or improve bus accessibility, journey times and resilience.
- 4 Dockless bike and scooter parking** Support the City with a review of the existing location of dockless bike and scooter parking and identify new parking opportunities to meet existing demand and reduce impact on those using the footways.
- 5 Traffic management** Explore changes to the highway network, whilst retaining appropriate access to streets and buildings for taxis and other essential vehicles.
- 6 Freight, service and delivery** Provide appropriate provision for kerbside requirements associated with FSD, while exploring efficiencies and consolidation in FSD activity.

- Recommendations for area-wide projects**
 - + Work in close collaboration with the City, TfL and other key stakeholders to deliver any changes to the highway network to ensure they align and complement City and mayoral policy, as well way any schemes currently being developed in the area.
 - + Encourage tenants to sign up for consolidated servicing schemes
 - + Work with the City as they develop the Fenchurch Street Healthy Streets Plan



Key findings from engagement

- + We heard that there is frustration with delivery vans blocking pedestrian routes, the lack of protected cycle lanes and congested streets.
- + "There are no protected cycle lanes other than on Camomile Street, which is just a few wands. Do you know how many vehicles block the cycle lanes daily?!!!"
- + "Monument junction is completely hostile to pedestrians or lingering. Too much traffic. Too busy. I just want to get out of there as soon as possible."
- + "Houndsditch is impossible to cross."
- + Ongoing construction work is a continuous nuisance.

Key findings from research

- + The streets between the main cycle corridors aren't well used by cyclists.
- + There is a lack of cycle facilities on streets with higher traffic flows, including Wormwood Street and Eastcheap.
- + The area's footways are incredibly congested at peak times.
- + Pedestrian and cycle casualties account for 71% of all casualties in the EC BID area over the last 5 years.
- + Construction features are endemic to this area, encroaching on the public realm experience, and will remain omnipresent over the years.

→
See Evidence Base for full transport study and analysis

Aim 6: An accessible mobility environment for all

1 Walking and wheeling

Widen pavements at key locations, introduce new pedestrian crossings, and enhance pedestrian priority on local streets.



Many of the pavements across this area of the City are narrow, allowing more room for cars than people. Though some streets must retain multiple lanes of vehicle traffic and parking, there is scope to calm traffic and improve the pedestrian experience along a handful of major streets across the area. Providing wider footways and safer crossing opportunities can deliver a better balance between motor vehicles and all other uses of the street environment, and introduce opportunities to enhance the public realm. The volume of pedestrians using the streets within the EC BID area greatly outnumber the volume of motor vehicles, which should be reflected in the streetspace and public realm environment.

fig. 65 Kerb cuts are not consistently deployed across the BID area.

2 Cycling

Improve provision for safe cycling on streets with high volumes of motor vehicles and enhance permeability on quiet routes. Provide additional cycle parking where possible.



Active travel through cycling and wheeling should be prioritised as it is an accessible and sustainable mode of mobility. Motor vehicle volumes across many of the streets with the EC BID area are low due to recently implemented schemes (such as Bank junction and Bishopsgate Streetspace), which can provide quiet routes for cyclists. Protected cycle lanes should be considered on streets with higher motor vehicle volumes including Eastcheap, Bevis Marks and Houndsditch. These routes would link up with the London-wide cycle network.

New or increased cycle parking should be provided in locations where demand exceeds supply and at popular visitor destinations.

fig. 66 Gracechurch Street and Bishopsgate are challenging cycling environments

3 Public transport

Ensure proposed schemes maintain or improve bus accessibility, journey times and resilience.

Any proposed scheme will complement existing bus priority measures, including traffic management that reduces other motorised vehicles.

Aim 6: An accessible mobility environment for all

4 Dockless bike and scooter parking

Support the City with a review of the existing location of dockless bike and scooter parking and identify new parking opportunities to meet existing demand and reduce impact on those using the footways.

Working with the City of London, and their on-going review, to provide official dockless parking spaces where there is currently little or no provision on Bishopsgate, Philpot Lane, St Mary Axe, and Cornhill.



fig. 67 Dedicated parking zones for dockless bikes and electric scooters are well-used and need expanding across the area to avoid overcrowding in some areas

5 Traffic management

Explore changes to the highway network, whilst retaining appropriate access to streets and buildings for taxis and other essential vehicles.

Movement and access on the highway network will be reviewed to ensure it provides the best possible public realm experience and a safe space for those walking, wheeling and cycling. This is a critical step toward improved experience for walking, wheeling and cycling, and achieving Net-Zero and Vision Zero. This may include vehicle restrictions at certain times of the day, and timed use of kerbside loading and parking bays. Any changes would ensure local businesses can operate efficiently, as well as providing access for motor vehicle when and where essential.

6 Freight, service and delivery (FSD)

Provide appropriate provision for kerbside requirements associated with FSD, while exploring efficiencies and consolidation in FSD activity.

FSD vehicles form up to 30% of motorised vehicles in the morning, reducing to 10% towards the evening, meaning they have a significant impact on traffic congestion, road safety, emissions and conflict with other street uses.

Reducing the volume and improving the management of moving and stationary FSD vehicles has the potential to significantly reduce the impact of motor vehicles on streets with the EC BID area, which provides the opportunity to reallocate road space to increase the level of amenity for walking, wheeling and cycling and to enhance the public realm.

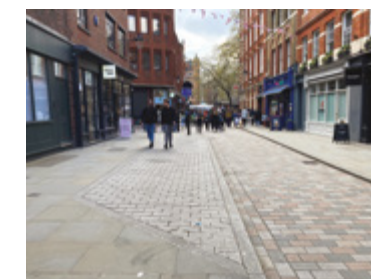


fig. 68 Servicing and loading on St Mary Axe

fig. 69 Raised loading pads on Earlham Street support pedestrian movement outside servicing hours

A safe and inclusive neighbourhood

Promote streets and spaces where all users feel safe and share in a sense of belonging.

Guiding principles

1 Diverse retail	Encourage a more diverse array of shops and services, including more affordable shopping and dining options.
2 Creative production	Encourage uses that are productive rather than for consumption, like spaces for cultural or after-work activities and events.
3 Play	Bring joy to the Eastern City through recreation areas for children and playful installations that are multifunctional and broadly inviting.
4 Public toilets	Encourage the provision of public toilets in new developments.
5 Recognise and reimagine the past	Unveil and reveal histories that invite a more diverse group of people into the area's past and present.
6 Care and belonging	Listen to lived experiences and needs of all people in the area to co-create the City as a place of care and belonging.

Recommendations for area-wide projects

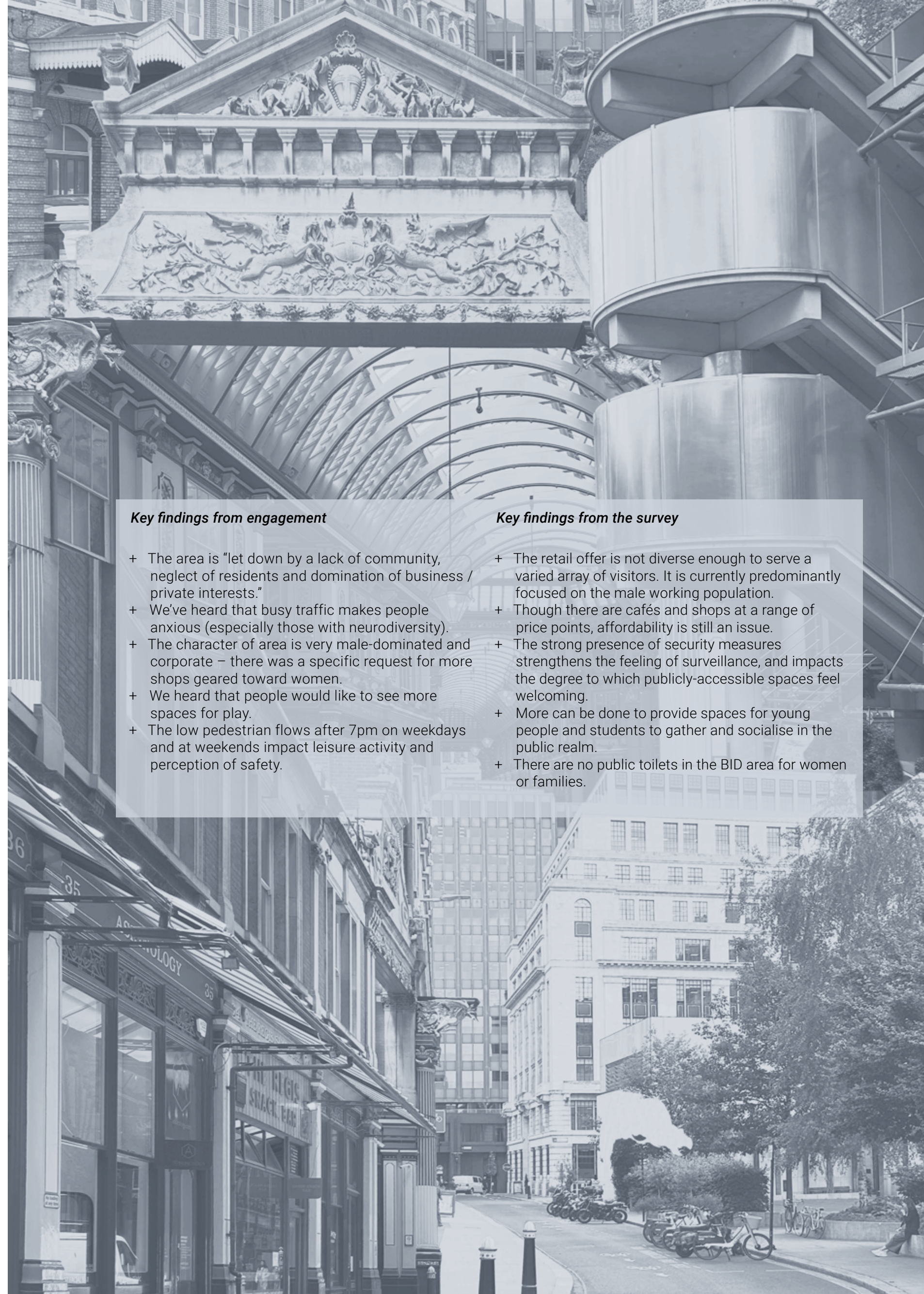
- + Sponsor a women's safety audit
- + Work with local businesses to understand their needs for later opening hours
- + Work with property owners to explore provision of toilet facilities within buildings
- + Support an artist to explore how to reframe the area's challenging histories
- + Commission a ground floor use strategy that looks at future retail needs (in partnership with CoL)

Key findings from engagement

- + The area is "let down by a lack of community, neglect of residents and domination of business / private interests."
- + We've heard that busy traffic makes people anxious (especially those with neurodiversity).
- + The character of area is very male-dominated and corporate – there was a specific request for more shops geared toward women.
- + We heard that people would like to see more spaces for play.
- + The low pedestrian flows after 7pm on weekdays and at weekends impact leisure activity and perception of safety.

Key findings from the survey

- + The retail offer is not diverse enough to serve a varied array of visitors. It is currently predominantly focused on the male working population.
- + Though there are cafés and shops at a range of price points, affordability is still an issue.
- + The strong presence of security measures strengthens the feeling of surveillance, and impacts the degree to which publicly-accessible spaces feel welcoming.
- + More can be done to provide spaces for young people and students to gather and socialise in the public realm.
- + There are no public toilets in the BID area for women or families.



1 Diverse retail

Encourage a more diverse array of shops and services, including more affordable shopping and dining options.



Local residents, visitors, and workers will all be more likely to spend more time in the Eastern City area if the retail offer matched their needs. Younger workers and female workers, as well as visitors and nearby residents, would be more inspired to spend time in the area if there were shops that addressed their needs. From background research, and from our engagement sessions, we know that more affordable food options and stores that consistently stock women's clothes are most in demand. (We heard a specific need for stores that sell tights!)

The BID can build on the existing evidence base supporting diverse retail and help develop a retail strategy for this area of the City.

fig. 70 A pub across from a men's tailoring shop describes the retail makeup of this part of the City.

2 Creative production

Encourage uses that are productive rather than for consumption, like spaces for cultural or after-work activities and events.



→
See Evidence Base for map of cultural infrastructure

Cultural planning often divides cultural uses into spaces of 'cultural consumption,' like cinemas, museums, and galleries, and spaces of 'cultural production' like artist workspaces, pottery studios, and other places where art is made rather than seen. This area of the Square Mile has very few spaces of either type, but there is a specific opportunity to encourage creative production in this area. Such uses would draw on the area's history of craft and commerce, while also encouraging people to come into the area on weekends and stay later on weekdays for creative activities.

fig. 71 Creativity lives in the City if one looks closely enough for it.

3 Play

Bring joy to the Eastern City through play equipment and playful installations.



This part of the City is, perhaps surprisingly, a magnet for families. On the weekends, it isn't too crowded, there aren't many cars, and there are squares and plazas for children to play in. Building play equipment, or even installations that encourage play and interaction, would support this emerging trend, and welcome more families and young people into the area. In turn, this would provide more footfall for businesses to stay open later (see Aim 3) and bolster the business case for more diverse retail and food options in the area.

fig. 72 The BID has sponsored playful installations in Leadenhall Market and this one in Fen Court that encourage children to come and explore this part of the City.

4 Public toilets

Encourage the provision of public toilets in new developments and opening existing facilities to the public.

It is remarkable that in a global business district of international stature, that is on the doorsteps of the nightlife district of Shoreditch and the nation's tenth-most visited tourist attraction at the Tower of London, there is not a single public toilet facility for women and families. The one public toilet in the BID area is a male-only facility. This needs to be addressed to create a welcoming and inclusive public realm.

Additional facilities can be provided in new developments, but the opening up of facilities in existing buildings should also be explored.

5 Recognise and reimagine the past

Unveil and reveal histories that invite a more diverse group of people into the area's past and present.



fig. 73 *The Battle Is Joined*, by Karyn Olivier, in Philadelphia seeks to subvert the traditional form of a plinth by introducing viewer's reflections into it. Image copyright: Michael E Reali

As London's centre for shipping, trade, and commerce for over four centuries, it is inevitable that the Eastern City's built fabric holds the darker sides of that history. Such histories can leave echoes in the architecture, and can make spaces feel forbidding and unwelcoming, especially to people from historically marginalised communities. Working alongside landowners, the Corporation, developers, and historians, the BID can help support the unveiling of these histories to create a more equitable and welcoming place.

6 Care and belonging

Listen to lived experiences and needs of all people in the area to co-create the City as a place of care and belonging.

The industries and spaces that define this area of the City have long been male-dominated and predominantly white. These demographics are beginning to change, with a younger and more diverse workforce, but active efforts should be taken to ensure that all people feel welcome and safe.

The City of London's Violence Against Women and Girls strategy identifies inadequate reporting as one of the main reasons why lived experiences and needs of women and girls are not fully understood. This reporting issue is exacerbated in the Eastern City area, given that there are many more workers and visitors than there are residents, who often are integral in holding perpetrators to account.

The BID is well-positioned to help foster these conversations and initiatives that can increase the feeling of belonging in public space, for example, the creation of an evidence base centring the lived experiences of a diverse set of people in the area.

Project families and site-specific interventions

Public realm improvements along principal routes

Five project families

To reflect the many functions, scales, and environments of the ground floor, and building on feedback from the engagement process and our extensive research, we have developed five families of projects, each focusing on different routes through the Eastern City area.

Within each project family, we have highlighted:

- **Small-scale interventions**
We have highlighted numerous small-scale interventions along each route, which align to the Aims of the Vision.
- **Priority projects**
Within each project family, we have highlighted discrete projects at specific site locations. These vary in scale and type, but all have identifiable critical pathways for delivery.
- **Transformational projects**
Across the families we have highlighted four larger-scale projects that can transform specific streets and spaces.

The projects have been selected through an analysis of what is already being funded or delivered by the City or by private developers, and those interventions that would have a high impact on the quality of the public realm across the BID area.

Gracechurch Street and Bishopsgate provide a good example of how priority projects were selected: TfL is already leading the improvement project along this critical route, which includes widening pavements and calming vehicle traffic, making safer conditions for both cyclists and pedestrians. Given that this key route is already being improved, we have omitted it in our project families.

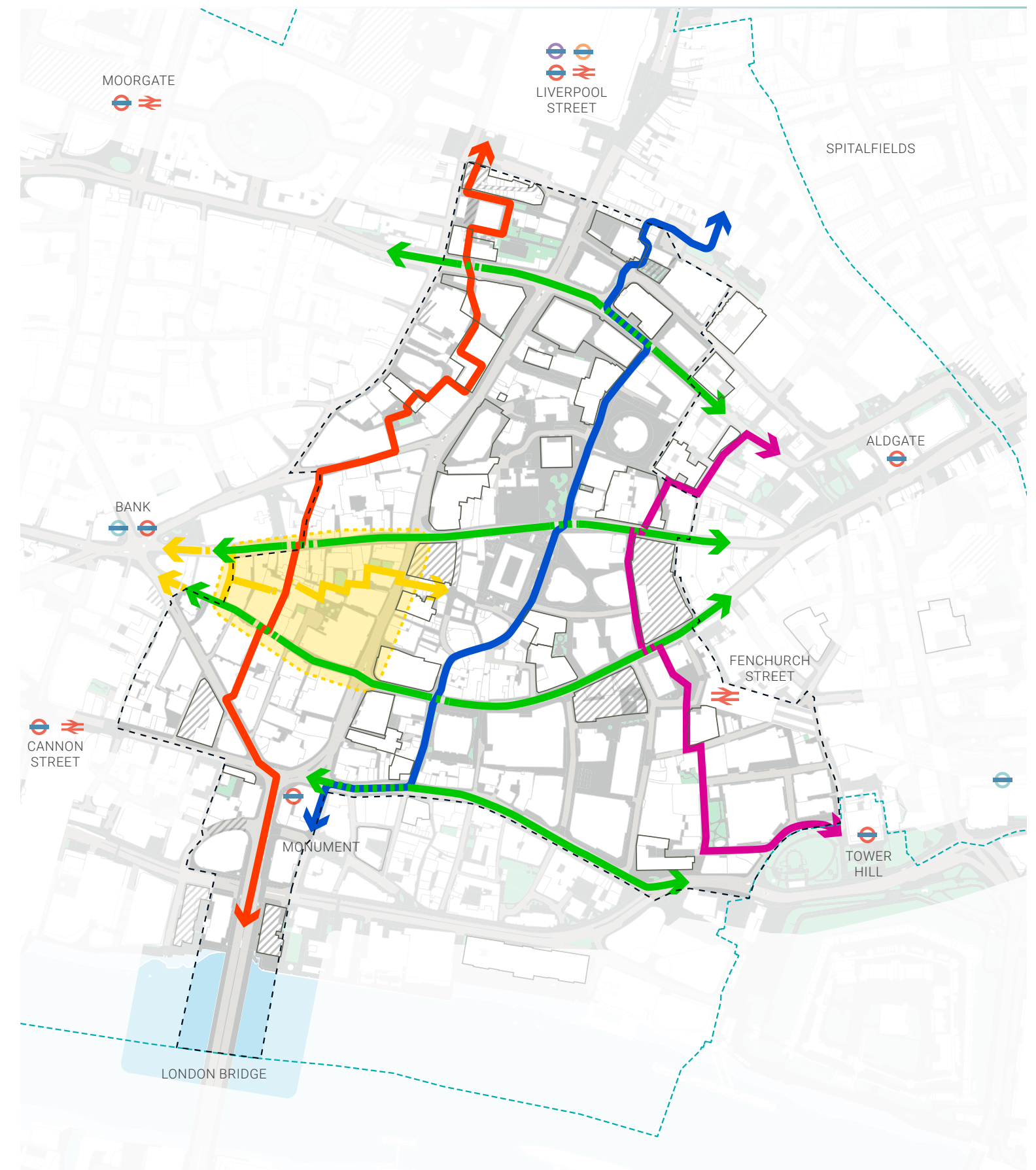
London Bridge ————— Liverpool Street

Monument ————— Spitalfields

Tower Hill ————— Aldgate

West ————— East

Bank ————— Leadenhall Market



- 1 The route from London Bridge to Liverpool Street, running north to south on the western side of Gracechurch Street and Bishopsgate.
- 2 The route from Monument to Spitalfields, running north to south on the eastern side of Gracechurch Street and Bishopsgate.
- 3 The southeastern gateways of the area, connecting Aldgate, Tower Hill, the Tower of London, and the riverfront to the Eastern City area.
- 4 The east-west routes that run through the area, connecting Aldgate to Bank and Fleet Street beyond.
- 5 The area's historic core, the remaining blocks of medieval courts, Victorian shopfronts and the alleyways that connect them.

Overview

The primary route from London Bridge to Liverpool Street Station follows the central spine of Gracechurch Street and Bishopsgate; but as a pedestrian, these streets can be overwhelming and uninviting, with heavy traffic and narrow pavements. This first route offers an alternative way, using historic lanes and alleyways to provide a pleasant journey between two important transit hubs.

RECENT AND UPCOMING PROJECTS

- A** Monument Junction improvements (TfL)
- B** New Bank tube entrance (TfL)
- C** Improvements to Abchurch Lane from TfL scheme
- D** King William Street improvements (CoL)
- E** Royal Exchange plaza improvements
- F** New covered public space at 55 Bishopsgate
- G** New connections through 55 and 99 Bishopsgate
- H** New cultural attraction and food market at 99 Bishopsgate
- I** New connection through 55 Old Broad Street
- J** New food market at Liverpool Street

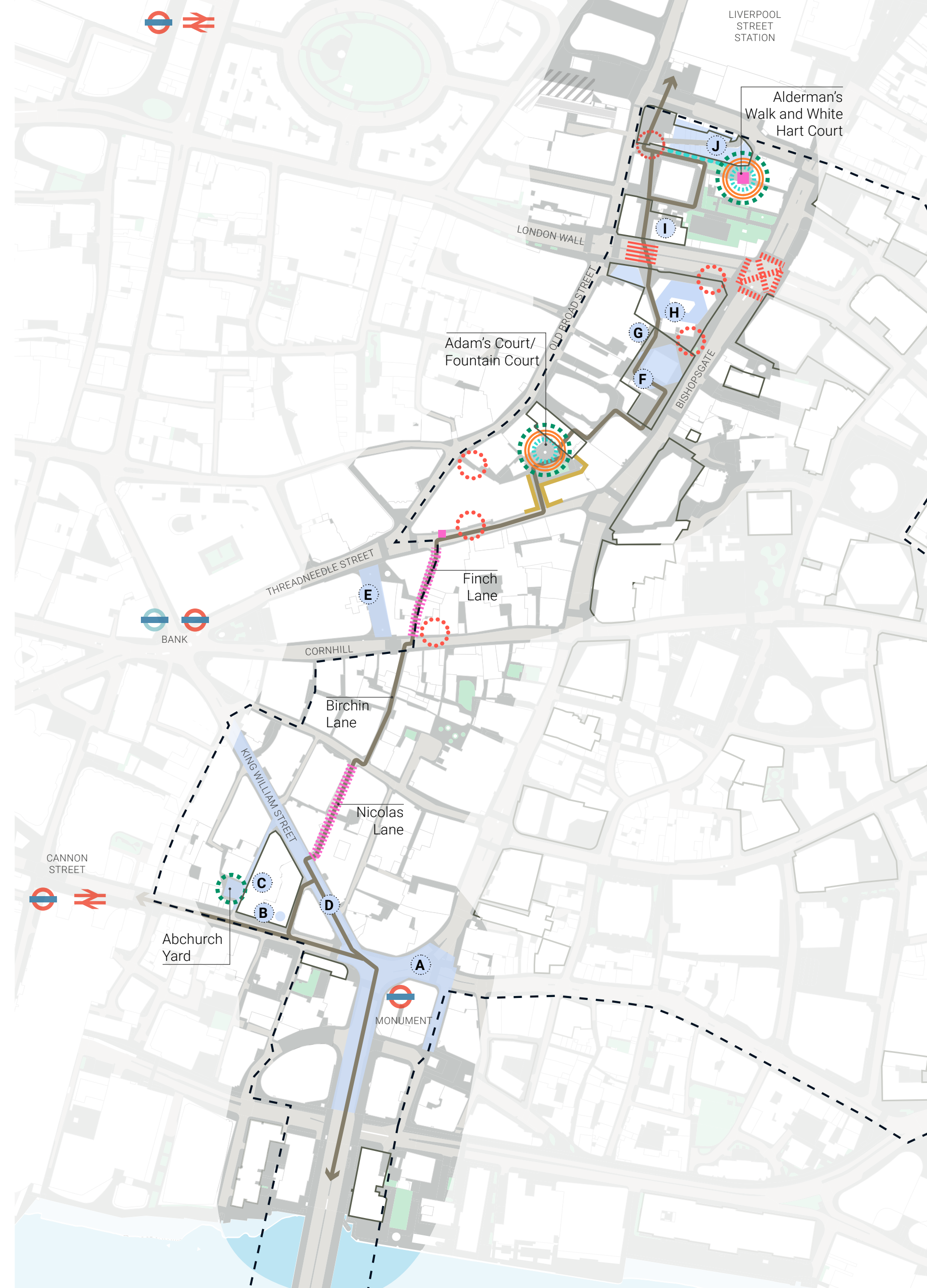
□ Development pipeline

PROPOSED IMPROVEMENTS

- New or improved public space Welcoming public space
- Opportunity for publicly-accessible ground floor
- Opportunity for planting Resilient trees and planting
- Space for events Lively uses and activity
- Dressing up the street
- Wayfinding totem
- Opportunity for paved street Intuitive navigation
- Signpost to spaces at height
- Heritage facade lighting Historic character and lighting
- Alleyway lighting scheme
- Timed street closure
- New pedestrian crossing Accessible
- Space for dockless bike parking
- Opportunity to reveal history Safe and inclusive
- Opportunity for play

Heritage

- + This project family connects small-scale medieval courts through medieval lanes and alleyways. The southern portion of the route features Victorian-era street improvements: King William Street, in particular, was created as a new street connecting the new London Bridge to Bank junction during the latter half of the nineteenth century to improve traffic circulation. In this way, this route features the historic layering that is very typical of this area and unique in its the depth of those layers.
- + Nicholas Lane, Birchin Lane, Finch Lane and the rest of the network of small lanes running north to south represent streets which used to have active frontages and tradespeople carrying out business. Trade cards list small offices and workshops on these small streets; this particular piece of commercial heritage is now lost.
- + Now-gone coffee houses in the alleyways were important locations for the founding of the abolitionist movement.
- + This route crosses over the former location of the Roman Wall, connecting it to this deeper past, and linking to ideas of being within and without the City walls, a condition which defined much of medieval history.



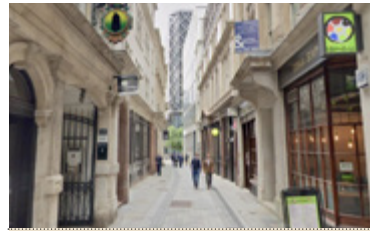
London Bridge

Abchurch Yard



One block west from the new Bank station tube entrance is a small courtyard alongside St Mary Abchurch, one of the churches in the BID area. Though most of the area is dedicated to tables and chairs for the neighbouring restaurant, the three benches that line the church wall stands as a reminder that even small pieces of public amenity add life to a space.

Nicholas Lane



St Swithin's Lane provides a good example of pavement and carriageway finishes that invite pedestrian movement.



With the opening of the new Bank Station entrance on the corner of Cannon Street and Nicholas lane, this alleyway will become a key alternative as a north-south pedestrian route. Nicholas Lane is lined with loading bays that create a back-of-house character. An area-wide consolidated servicing strategy will review servicing requirements for single buildings to introduce more active frontages within blank elevations and provide a more pedestrian-friendly environment.

Priority project

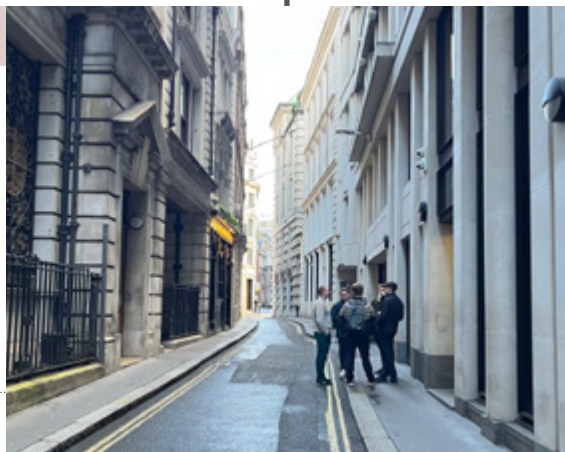
Transformational project
Lombard Street at Birchin Lane

(See following two pages)

Finch Lane



Marylebone Lane has a paved carriageway with additional space for seating and planting.



Similar to Nicholas Lane in character, Finch Lane is a narrow, one-way street running north-south between Cornhill and Threadneedle Street. Given its slightly more active frontage, and fewer loading entrances, Finch Lane could welcome additional amenity, including trees and seating.

Adam's Court/
Fountain Court



Additional seating, active uses, and a reconsidered, biodiverse planting scheme could create a new sense of place and unveil the potential of the central courtyard. Above, the Serpentine Gallery Pavilion.



Priority project

Adam's Court is a network of passages and courtyards leading from Threadneedle, Old Broad Street, Cornhill, and Bishopsgate into a central open space. As currently configured, there is a small lawn (which floods in heavy rains) and covered areas under arches. Separated from the main space by a balustrade and a slight level change is another area anchored by a central fountain. Existing mid-height planting blocks views into the central space.

It is adjacent to the Tower42 development site, which may introduce new active uses at ground floor and new routes through to the Tower 42 courtyard and Bishopsgate.

New developments
along the western side of
Bishopsgate

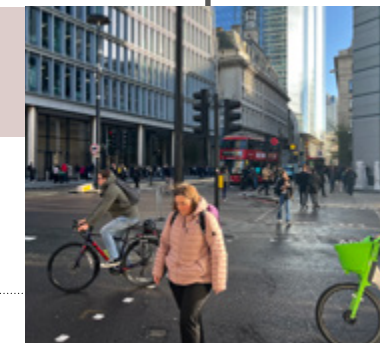


Two new major schemes will be developed in the coming decade along the western side of Bishopsgate. 55 and 99 Bishopsgate will not only bring new office space and new office workers, but will also introduce new permeability at ground floor. Thanks to a coordinated approach, these ground floor spaces connect to each other, creating a new route between Bishopsgate and Old Broad Street, improving pavement congestion at peak times.

Bishopsgate/
London Wall
junction



Shibuya-style diagonal crossings at Oxford Circus Junction.



The junction of Bishopsgate with Wormwood Street and Camomile Street has wide carriageways, which make crossing difficult, with an unpleasant environment due to noise and pollution. Overcrowded footways often encourage people to cross when it is not safe not to do so.

Diagonal crossings linking each corner of the junction would greatly improve pedestrian connectivity between Liverpool Street Station and the Eastern Cluster, and help to alleviate overcrowding and reduce pedestrian crossing times.

Alderman's Walk
and White Hart
Court



Seating nestled among trees and diverse planting beds at St Pancras Church invite conversation.

Priority project



Alderman's Walk and White Hart Court form a connected space that runs east-west between Bishopsgate and Old Broad Street behind St. Botolph-without-Bishopsgate church. There is an underused grassy area underneath three mature trees. The area receives reflected sunlight, and it is near the development site at 55 Old Broad Street as well as the new Boxpark food court on Liverpool Street.

This area could become a lovely lunchtime spot, with seating and biodiverse planting. Lighting the historic façades of the church would add character to the space, as could murals and artworks on the currently blank walls, which are also well suited for vertical planting or a mural.

There is an opportunity here to partner with Land Securities and other local stakeholders to explore shared aspirations for the site.

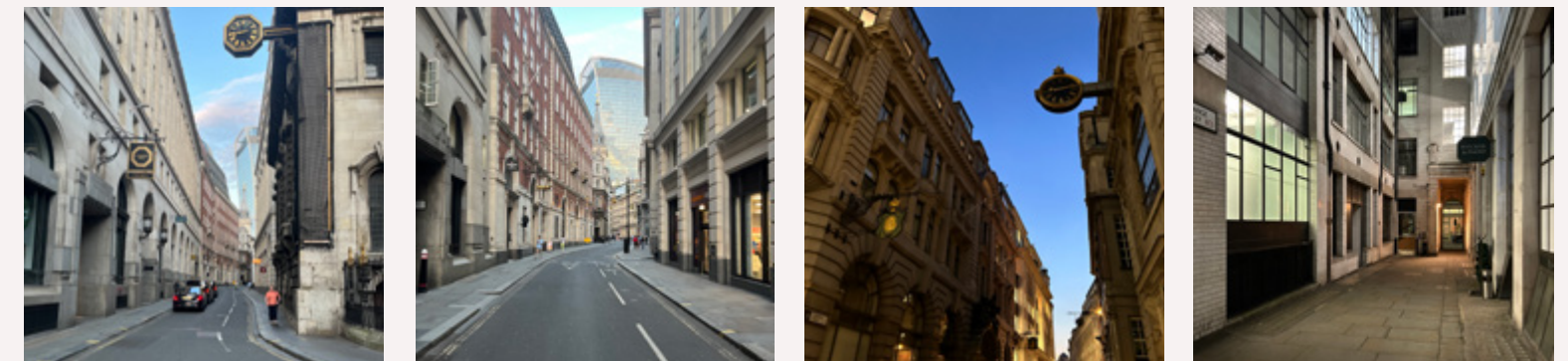
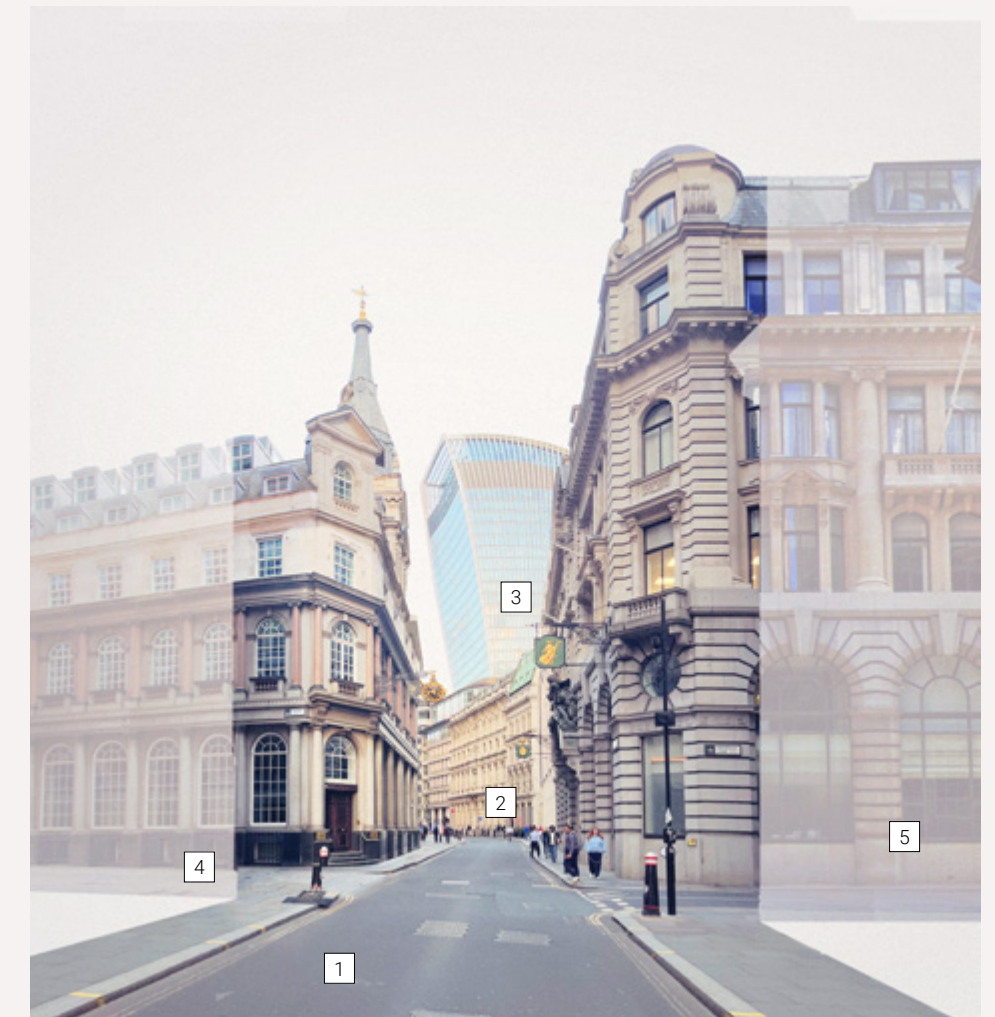
Liverpool Street

Lombard Street at Birchin Lane

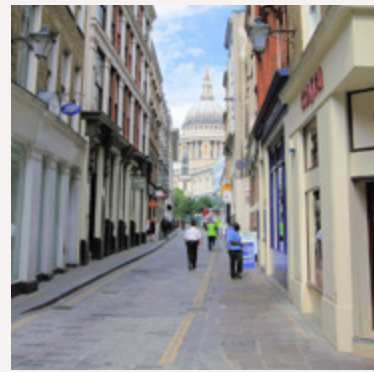
Existing condition

Lombard Street connects Bank to Gracechurch Street and is a quiet, low-traffic street. The heritage buildings and features, paired with the long view to the Walkie Talkie building, create a distinctive environment, specific only to this place in London.

- 1 Low motor vehicle traffic flow
- 2 Existing contra-flow cycle lane lacks clear demarcation from incoming traffic
- 3 Heritage signage contributes to the historic character of the area
- 4 Surface treatment on Birchin Lane supports implicit wayfinding
- 5 Nicholas Lane is used as a cut-through and is lined with blank frontages and loading bays



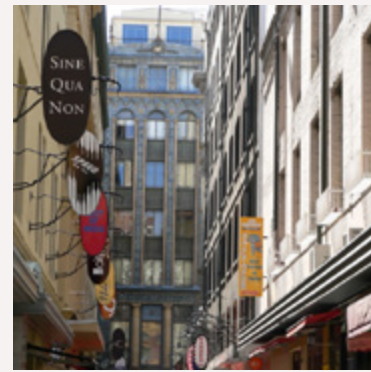
Lombard Street at Birchin Lane: Celebrating heritage



Paved carriageway in Watling Street



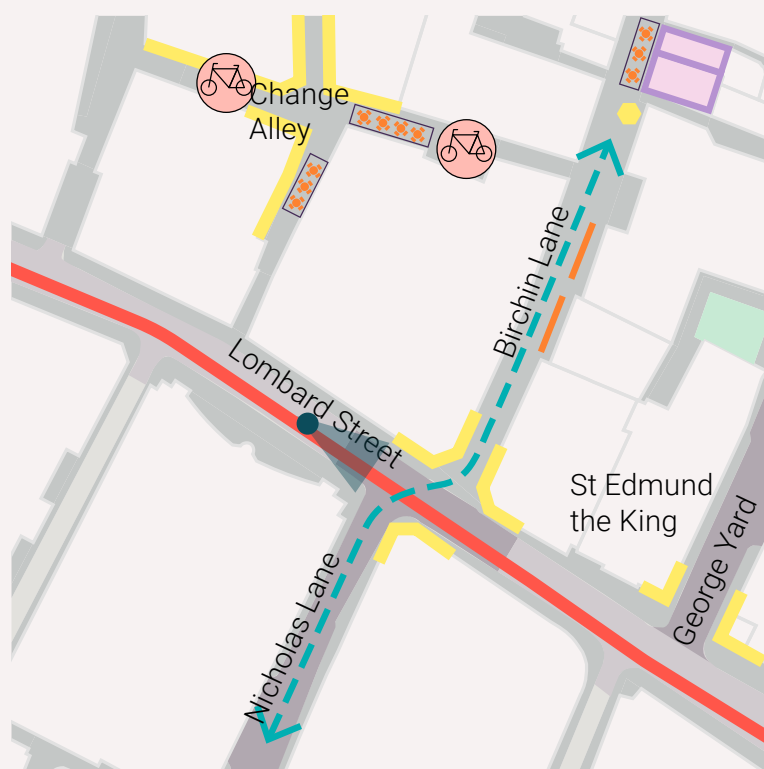
Lighting to architectural details and over public spaces in Glasgow



Coordinated signage in Degraves Street, part of Melbourne laneways

OPPORTUNITIES

- 1 Paved carriageway improves setting of heritage buildings
- 2 Raised carriageway at junction of Lombard Street, Birchin Lane and Nicholas Lane provides implicit wayfinding and supports secondary north-south pedestrian route
- 3 Improved cycling infrastructure with new markings to dedicated contra-flow cycle lane
- 4 Improved lighting and celebration of heritage features with additional signage
- 5 Additional seating to support active uses along Birchin Lane
- 6 Improved setting at Nicholas Lane with reduced loading access requirements following consolidated servicing strategy
- 7 Opportunity to use public art to celebrate abolitionist history at George Yard



- Active use
- Cycle lane
- Existing cycle parking
- Public art
- Paved carriageway
- Pedestrian flow
- Lighting to heritage façades
- Public seating
- Outdoor dining
- Viewpoint



Project stakeholders include City of London Corporation, local landowners and property developers

Overview The character of the spaces to the east of the central Bishopsgate and Gracechurch Street spine differs from its western counterpart, weaving through tall, contemporary office towers. Still, the route connecting the Monument with Spitalfields highlights smaller-scale spaces for dwelling and engaging with the commercial history of the area.

RECENT AND UPCOMING PROJECTS

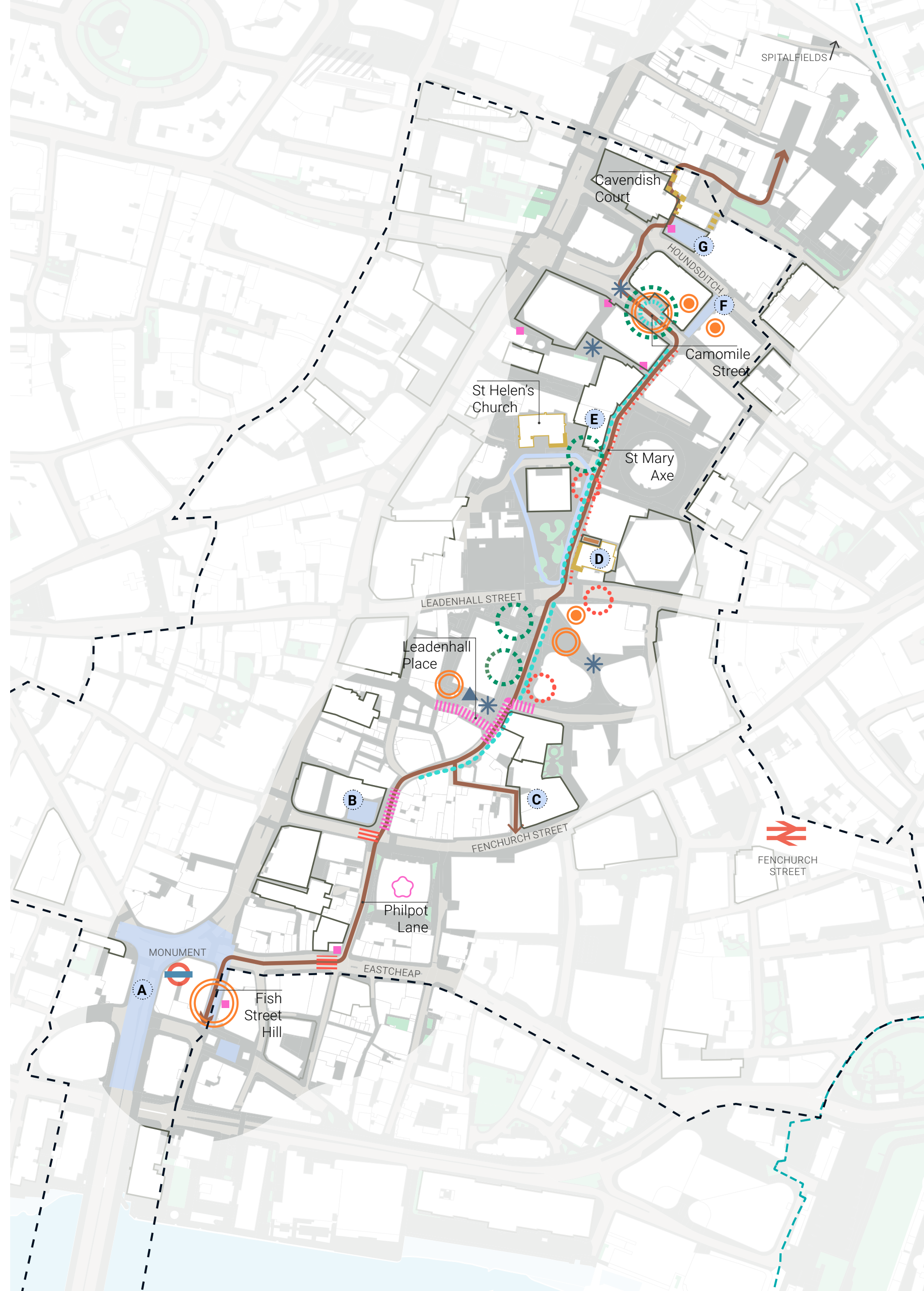
- A** Monument Junction improvements (TfL)
- B** Proposed food market at 70 Gracechurch
- C** Potential to improve Cullum Street through Fountain House development
- D** St Andrew's Undershaft churchyard improvements (CoL)
- E** Developments along western side of St Mary Axe
- F** Recently improved cut-through at St Mary Axe
- G** Jubilee Gardens (CoL)
- Development pipeline

PROPOSED IMPROVEMENTS

- New or improved public space Welcoming public space
- Opportunity for publicly-accessible ground floor
- Opportunity for planting Resilient trees and planting
- Space for events Lively uses and activity
- Dressing up the street
- Wayfinding totem
- Opportunity for paved street Intuitive navigation
- ☆ Signpost to spaces at height
- Heritage facade lighting Historic character and lighting
- Alleyway lighting scheme
- Timed street closure
- New pedestrian crossing Accessible
- Space for dockless bike parking
- ▲ Opportunity to reveal history Safe and inclusive
- * Opportunity for play

Heritage

- + This area of the Eastern City is characterised by the post-war redevelopment. This area was bombed heavily in the Blitz, and large land parcels that previously took centuries to assemble were assembled almost overnight, allowing for new types of development to occur. Large parcels allowed for towers and plazas, a typology favoured by modernist planners and architects. Post-war development paved the way for more iconic towers of various styles and the development of new plaza open spaces in the following decades.
- + Despite the contemporary nature of much of the architecture and urban pattern, Philpot Lane, Lime Street, St Mary Axe were all part of the medieval street pattern in the area producing a distinctively curved thoroughfare easy to pick out on a map.
- + This route is home to Lloyds of London, on the site of the historic location of the East India Company which shaped much of British imperial history.



Monument

Fish Street Hill



The Monument to the Great Fire of London is set back from the axis of London Bridge and Gracechurch Street, but it is visible looking south down Fish Street Hill, a semi-pedestrianised street that connects Eastcheap with the Monument. Not only does it afford a view to the Monument, but there is also an entrance to the Monument tube station here, and is a key arrival point to the BID area. Though the street is paved to a high quality, the public amenity here is minimal, amounting to just a few cycle stands.

The addition of seating and planting here would improve the arrival experience and provide a more welcoming atmosphere.

Philpot Lane



Philpot Lane links Eastcheap and Fenchurch Street. It is one-way northbound for motor vehicles. The entrance to the Sky Garden is located here, and its pavements are often crowded with tourists moving to and from this popular visitor attraction. Along the western side of the street there are small retail units, adding a sense of vitality to the street. At the top and bottom of Philpot Lane, there are landmark views up to the Lloyd's Building and the Gherkin and down to the Shard.

Philpot Lane provides a link for motor vehicles going from Monument junction to Fenchurch Street. Therefore, it cannot be fully closed to traffic, but there could be timed access to limit the amount of heavy goods vehicles that travel on this street. This would allow for the loading bays alongside the Sky Garden to be used by pedestrians during periods of busy pedestrian activity.



Loading pads on Bond Street allow the street to function differently at different times and for different uses and users.

Fenchurch Street



The loading bay at 70 Gracechurch Street can be used as a market during the day thanks to restricted loading times.



The western end of Fenchurch Street is wide with no protected pedestrian crossing. With the development of 70 Gracechurch Street, there is an opportunity to improve the pedestrian condition at this junction, and connect Sky Garden to the improved public realm at the northwest corner of Fenchurch Street and Lime Street.

Leadenhall Place



Light-touch improvements to Leadenhall Place could have a significant impact and improve the unique heritage setting of the space. Public realm improvements will need to balance pedestrian and servicing needs.



Priority project

Leadenhall Place is the 'back' entrance to Leadenhall Market, and provides service access to the office buildings along Lime Street. It connects to the Lloyd's lower ground floor area, which has the ambition to become a more welcoming publicly-accessible space. There is substantial heritage value to this site, as the former location of the East India House. The contrast of the Victorian aesthetics of Leadenhall Market and the postmodern steel of the Lloyd's Building is a perfect encapsulation of the layered built history of the EC BID area, and simple improvements could enhance it to high effect.

St Mary Axe



Raised loading bays on Sloane Street can be used as footway space outside loading times.



Priority project

St Mary Axe is a major pedestrian link for commuters travelling to and from Fenchurch Street Station and Liverpool Street Station. At peak commute times, the pavements can be incredibly crowded. In the mornings, especially, there is significant vehicle traffic, particularly of lorries and coaches, creating an unsafe pedestrian environment. Given the role it plays in servicing the office buildings, it is unlikely that St Mary Axe could be fully closed to traffic; but timed servicing restrictions could improve the pedestrian condition at peak hours, as could a concerted effort to consolidate servicing of the new developments along the western side of the street, and the use of innovative technologies such as virtual loading bays.



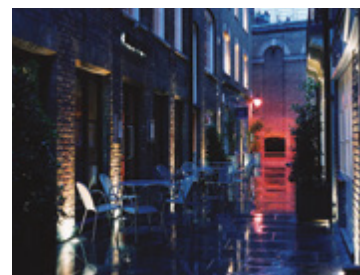
Virtual loading bays are being tested on Watling Street.

Transformational project

Camomile Street

(See following two pages)

Jubilee Gardens, Cavendish Court and Barbon Alley



Lancashire Court in Marylebone uses light to create an inviting atmosphere.



Liverpool Street

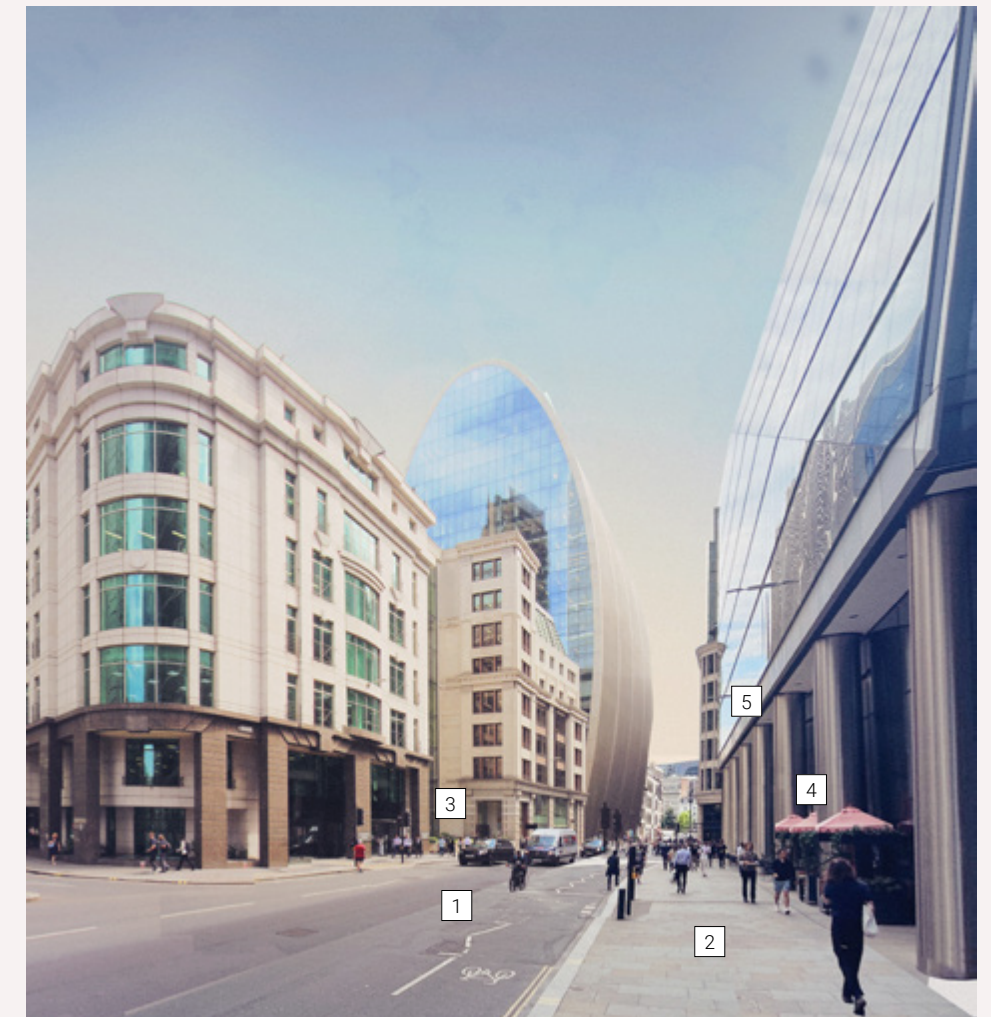
The City begins to transform into Spitalfields at Jubilee Gardens, and the connecting alleyways to Devonshire Row and Square, Cavendish Court and Barbon Alley. Jubilee Gardens is currently under construction, funded by the City of London with contributions from the BID. The narrow alleyways could also form part of a lighting pilot scheme to test ways in which lighting could contribute to a lively and safe feeling in the area.

Camomile Street

Existing condition

Camomile Street forms part of the east-west route that links London Wall to Aldgate via Houndsditch and Bevis Marks. It experiences high traffic volumes, including buses, heavy goods vehicles, taxis, and private cars, as well as cycles. Pedestrian flows are high due to the proximity to Liverpool Street Station, but there are just two pedestrian crossings. The pavements, though relatively wide for the area, can get highly congested at commute times and lunchtime. Bevis Marks and Houndsditch have multiple traffic lanes, with little opportunity to stop and rest, and minimal greening.

- 1 High motor vehicle flows and limited pedestrian crossing opportunities
- 2 Uneven footway allocation with one side considerably wider than the other
- 3 Lack of planting and areas for dwelling
- 4 Limited active uses and outdoor seating
- 5 The existing material palette is overwhelmingly cold and stark



Camomile Street: A new park in the City of London



The removal of Aldgate Gyratory unlocked the possibility for a vibrant new public space (Aldgate Square).



A new park was created at Strand Aldwych following the removal of vehicular traffic



Tree-lined boulevard established through the redevelopment of Harbour Way at Boston Seaport

OPPORTUNITIES

- 1 A transformational rearrangement of Camomile Street and Houndsditch creates the opportunity for a new piece of public space with significant greening. New planting beds with integrated seating and diverse mix of species provide spaces for dwelling
- 2 Removed gyratory system. Houndsditch reconfigured to allow two-way traffic
- 3 Proposed active uses support the activation of this new public space
- 4 Two-way cycle track through new public space
- 5 Vertical planting to building facade
- 6 Wayfinding totem and public art aid navigation across the area
- 7 Additional cycle parking along Bevis Marks, with areas set aside for bike and scooter hire parking
- 8 Widened and relocated crossing improves connectivity with Liverpool Street Station
- 9 Play features that evoke London Wall heritage

The EC BID would seek to complement the City's work on the London Wall Corridor study, with a specific feasibility study for a public space at Camomile Street.



- Existing tree
- Proposed tree
- Active use
- Cycle lane
- Existing cycle parking
- Proposed cycle parking
- Public art
- Paved carriageway
- Wayfinding totem
- Crossing
- Play
- Viewpoint

Project stakeholders include City of London Corporation, local landowners and property developers, TfL



Overview

The Eastern City is within a ten-minute walk of Whitechapel and the Tower of London, sites of enormous cultural and historic significance to tourists and residents. Strengthening the physical connection between the Eastern City and these areas to the southeast starts by improving the experience of arriving to the area from the Tower of London, from Fenchurch Street Station, and from Aldgate.

RECENT AND UPCOMING PROJECTS

- A** Vine Street improvements (Aldgate BID)
- B** New, active ground floor at New London House
- C** New route through 50 Fenchurch Street
- D** Improved crossing of Leadenhall Street through CoL scheme
- E** New route through 100 Leadenhall
- F** Creechurch Lane improvements (CoL)

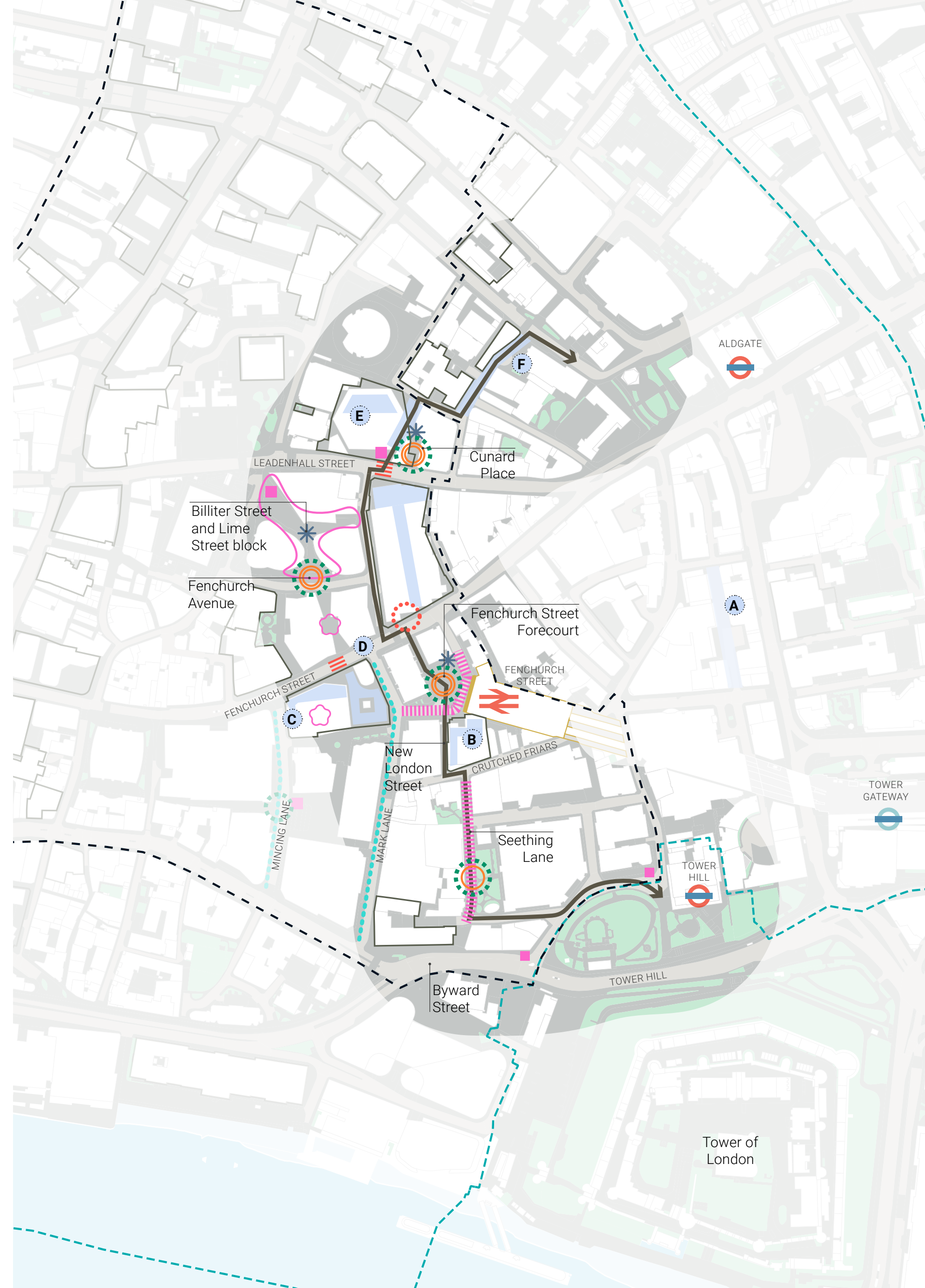
□ Development pipeline

PROPOSED IMPROVEMENTS

- New or improved public space Welcoming public space
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Heritage

- + Remnants of the Roman Wall near Tower Hill Station connect this area to the founding of Londinium. The Tower of London, arguably one of the nation's most important built heritage assets, is just a five-minute walk from the edge of the BID area.
- + Fenchurch Street Station and the rail lines connecting it eastward were part of a series of transportation improvements made in the mid- and late nineteenth century that began to change the nature of the City from a dense mix of uses to a predominantly commercial area, with few residents. Train lines like the one into Fenchurch Street station allowed people to live further away from their jobs.



Tower Hill

Byward Street

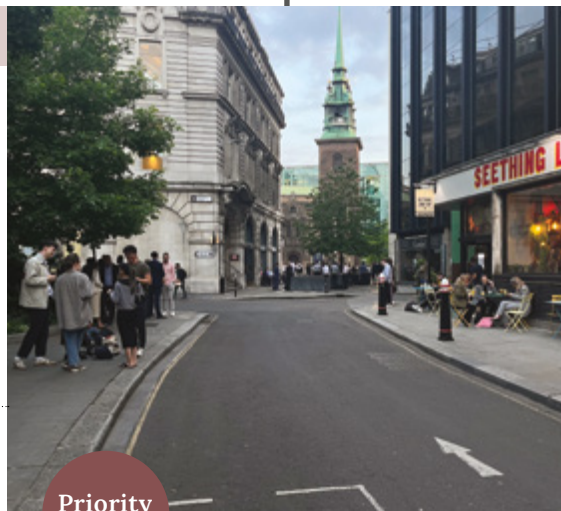


Byward Street connects the Eastern City to the Tower of London, and is a heavily trafficked street with a segregated two-way cycleway. The pavements are relatively wide, and get direct sunlight throughout the year, but the navigational experience of getting from the Tower of London across the street can be confusing. There is an opportunity to install Legible London totems along this street, and perhaps add some larger-scale graphics where the setting is appropriate.

Seething Lane



Improvements to the carriageway and built-in flexibility of footways can contribute to creating a more accessible and welcoming streetscape.



Priority project

Seething Lane connects Byward Street to Fenchurch Street Station, and is anchored by the churches of St Olave's to the north and All-Hallows-by-the-Tower to the south. Along most of the eastern side of the street is Seething Lane Gardens, a sheltered and lushly planted space at the back of the Four Seasons Hotel. People standing outside the pub at the end of the street often add to the setting, making the street feel active and lively. This area is part of the City of London's Fenchurch Street Healthy Streets Plan, which is currently in development.

There is an opportunity to build on the successful City-led improvements to Seething Lane Gardens and expand the high-quality public realm to the pavements and carriageway. A paved carriageway with raised loading and parking bays would support public life, particularly at the southern end of the street.

New London Street



New London House is currently in planning to become a Premier Inn. A more active ground floor at Fenchurch Street Station will have a positive impact on that space, but the building's eastern and southern façades have an important role to play in establishing a connection between Seething Lane and Fenchurch Street.

There is a short staircase here but the lift is often out of order, creating a barrier to movement for people in wheelchairs and those pushing buggies. A concerted effort should be made to improve this persistent accessibility issue.

Transformational project
Fenchurch Street Station

(See following two pages)

50 and 120 Fenchurch Street



The new public space at 50 Fenchurch will improve pedestrian permeability and create a direct connection to the covered public route at 120 Fenchurch Street.

Fenchurch Avenue



Timber slats integrated to stepped seating blocks for increased comfort



The existing black granite seating along the northern pavement of Fenchurch Avenue is generous in scale, but could be improved with some material changes that increase comfort. The line of trees recently trimmed back could be replaced with planting that is more suitable to the windy conditions at the base of the tower.

The Billiter Street, Fenchurch Avenue, Lime Street, and Leadenhall Street block



Crossing Fenchurch Street from the station leads into a network of tall buildings, lanes and pedestrianised plazas. The block between Billiter Street, Fenchurch Avenue, Lime Street, and Leadenhall Street is easy to pick out on a map thanks to its distinctive collection of three towers (The Scalpel at 52-54 Lime Street, the distinctively concave Willis Building at 51 Lime Street, and 25 Fenchurch Avenue). However, the entirely pedestrianised routes through this block are unnamed, standing in stark contrast to the characterfully named medieval alleyways to the west of Gracechurch Street. While a name would not meaningfully change the physicality of this space, it would lend a sense of place to an otherwise relatively sterile place.

Cunard Place



Cours du Marais in Paris uses simple movable furniture in earth tones to invite people to spend time in the space.



Plans developed by the City of London will improve Leadenhall Street with pavement widening, new seating and new planting. These improvements will make crossing the street much easier, and create new spaces for incidental use along it. Leading north from Leadenhall Street is Cunard Place, a pedestrian court that connects to Bury Street. It is currently home to a Sculpture in the City plinth and a handful of cycle stands, and sees a degree of foot traffic at peak times. The development of 100 Leadenhall, will introduce two new pedestrian routes between Bury and Leadenhall Streets, in keeping with City development guidance. These new routes create an opportunity to make Cunard Place a space primarily for dwelling, rather than passing through. New seating, planting, and visual activation here would make a new piece of incidental public space.

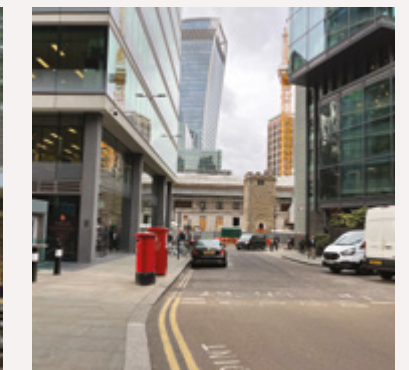
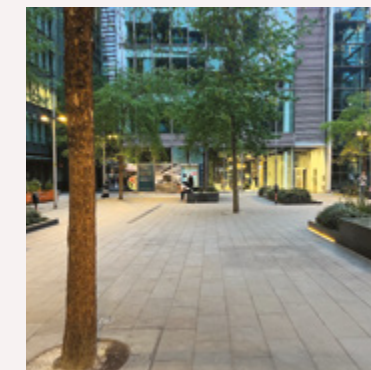
Aldgate

Fenchurch Street Station

Existing condition

Fenchurch Street Station opens up onto a sizeable public space, owned by Network Rail and left largely open to allow for pedestrian traffic to the station. Recently, the BID added terracotta planter benches along some active frontages. Every Tuesday to Friday, a lunchtime food market attracts workers from the surrounding office buildings. The hotel development at New London House will open up onto the space, which will make the space feel more active later into the evening.

- 1 Existing planting beds lack a diverse mix of planting and do not provide visual interest.
- 2 Existing granite benches feel cold and wet in the winter months
- 3 Lunchtime market is popular and would benefit from a dedicated space
- 4 Existing lamp columns are outdated and do not provide adequate lighting levels to create a safe space at night



Fenchurch Street Station: A safe and inviting gateway



Arrival square with paved carriageway at the Place d'Armes, Montreal



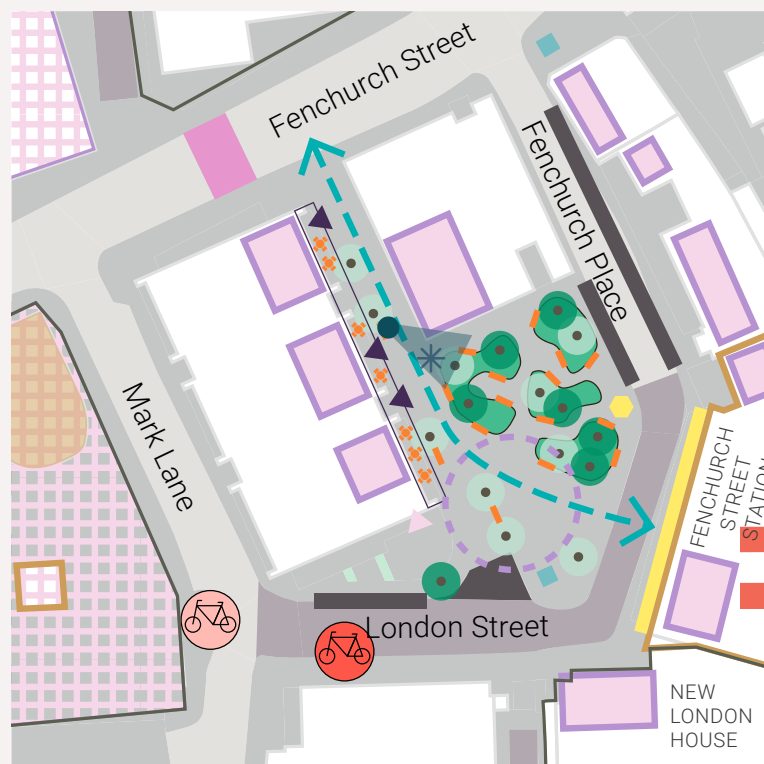
Curved planting beds in Exchange Square



Clear and decluttered route on arrival from Birmingham's station

OPPORTUNITIES

- 1 Reconfigured in-ground planters with biodiverse planting, improved seating and low-level lighting create a more welcoming space for dwelling
- 2 Improved route-through to the station
- 3 Paved and flush carriageway for an improved arrival experience along Fenchurch Place and London Street
- 4 Uplighting to station facade highlighting heritage features
- 5 Decluttered public space to host weekly lunchtime market
- 6 Existing trees retained
- 7 Retained taxi rank
- 8 Proposed hotel lobby and meanwhile active uses support the activation of the new piazza throughout the day and evening
- 9 Opportunity for playable features



- Existing tree
- Proposed tree
- Active use
- Existing cycle parking
- Proposed cycle parking
- Public art
- Paved carriageway
- Wayfinding totem
- Crossing
- Play
- Pedestrian flow
- Lighting to heritage façades
- Public seating
- Outdoor dining
- Loading and servicing

Viewpoint

Project stakeholders include City of London Corporation, local landowners and property developers, Network Rail



Overview The streets that run west-to-east across the BID area form a distinctive block pattern that dates back to medieval times. While they perform a similar function of connecting east and west sides of the City, each has a different character, which can be enhanced with specific improvements.

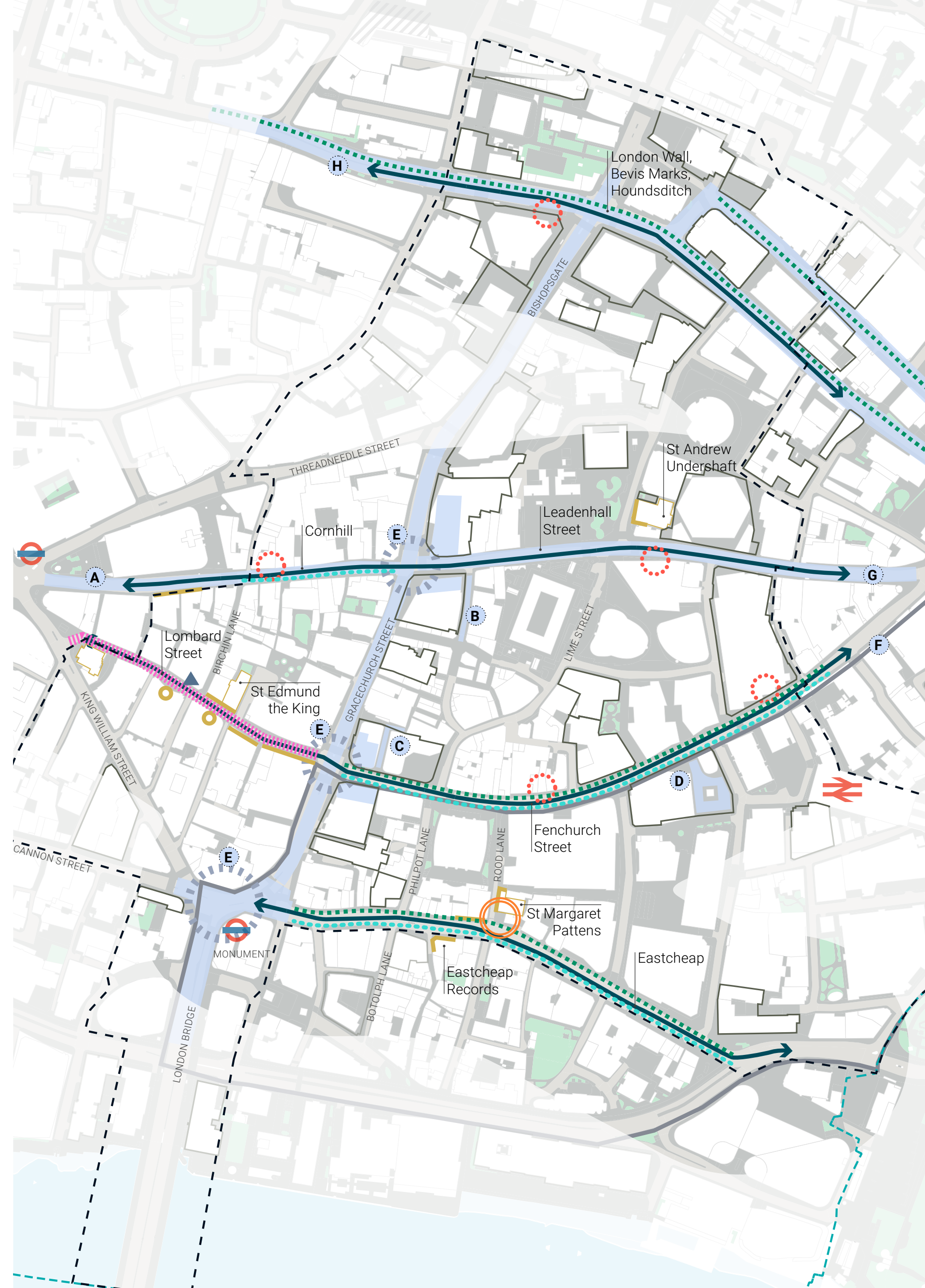
RECENT AND UPCOMING PROJECTS

- A** Cornhill traffic scheme (CoL)
- B** New route through 1 Leadenhall Street
- C** New route through 70 Gracechurch Street
- D** New route through 50 Fenchurch Street
- E** Junctions improvements through Monument Junction and Gracechurch/ Bishopsgate schemes (TfL)
- F** Fenchurch Street Healthy Streets Plan (CoL)
- G** Leadenhall Street improvements (CoL)
- H** London Wall study (CoL)
- Development pipeline

PROPOSED IMPROVEMENTS

- | | | |
|---|--|---------------------------------|
| ○ | New or improved public space | Welcoming public space |
| ● | Opportunity for publicly-accessible ground floor | |
| ○ | Opportunity for planting | Resilient trees and planting |
| ○ | Space for events | Lively uses and activity |
| — | Dressing up the street | |
| ■ | Wayfinding totem | |
| — | Opportunity for paved street | Intuitive navigation |
| ☆ | Signpost to spaces at height | |
| □ | Heritage facade lighting | Historic character and lighting |
| — | Alleyway lighting scheme | |
| — | Timed street closure | |
| — | New pedestrian crossing | Accessible |
| ○ | Space for dockless bike parking | |
| ▲ | Opportunity to reveal history | Safe and inclusive |
| * | Opportunity for play | |

Heritage + These east-west streets create a unique street pattern, dating back to the medieval period. Historically there was a prevalence of banks along these streets, which speaks to the unique commercial identity of this part of the City. The signage along Lombard Street and Cornhill are reminders of the smaller banks that defined commercial banking during the seventeenth and eighteenth centuries.



West

London Wall, Bevis Marks, Houndsditch



The rerouting of vehicular traffic at Strand Aldwych created additional space to move and gather with planting and seating.



London Wall is named for the former Roman wall that surrounded the City until the medieval period. It is an important east-west artery that connects Whitechapel and East London to the City and the rest of Central London. The City of London is currently studying how traffic on this street could be calmed to accommodate more space for pedestrians and cyclists, as well as additional planting and seating. Improvements to London Wall could have an incredible impact on the Eastern City by relieving congested pavements, by adjusting traffic flows, and by potentially creating a new piece of public realm at Camomile Street.

Cornhill to Leadenhall Street



Cornhill and Leadenhall Street form a commercial spine of the City of London, connecting Cheapside to Aldgate and Whitechapel. Improvements to Leadenhall Street are currently in the design stage as part of the City Cluster Healthy Streets Plan. There are opportunities to support the City of London as it explores potential improvements to Cornhill as part of a future Bank and Guildhall Healthy Streets Plan.

One Leadenhall



The recent development at One Leadenhall will open up new pedestrian connections from Leadenhall Street to Leadenhall Market.

Lombard Street



The signage lanterns of Dubrovnik's old town enhance the area's character during the day and at night.

Priority project



Lombard Street connects Bank to Gracechurch Street, and is a quiet, low-traffic street. The heritage buildings and features, when paired with the long view to the Walkie Talkie building, creates a distinctive environment, specific only to this place in London.

The heritage buildings and features along Lombard Street could be enhanced through lighting. The legibility of the existing cycle lane could also be improved. Its proximity to George Yard makes it a perfect location for a public art project that celebrates the abolitionist heritage of the area, drawing on the heritage vocabulary of the signage to elevate the important work carried out by formerly enslaved people alongside British abolitionists.

Fenchurch Street



Sauchiehall Street has generous pavement that can accommodate pedestrian movement alongside trees and seating.

Priority project



Fenchurch Street is a two-way street that sees moderate levels of traffic throughout the weekday and weekends. It is within the City's Fenchurch Street Healthy Streets area, and has been highlighted in the City's transportation strategy as a proposed priority location of 'safer streets,' which will aim to reduce the likelihood and severity of collisions. It is a City Access street which means that it is a key vehicle connection for local traffic.

Where Eastcheap is primarily focused on movement, there is an opportunity to establish a carriageway with a fixed width along Fenchurch Street, creating more space on the pavements for pedestrians and amenity including seating and planting.

Transformational project

Eastcheap

(See following two pages)

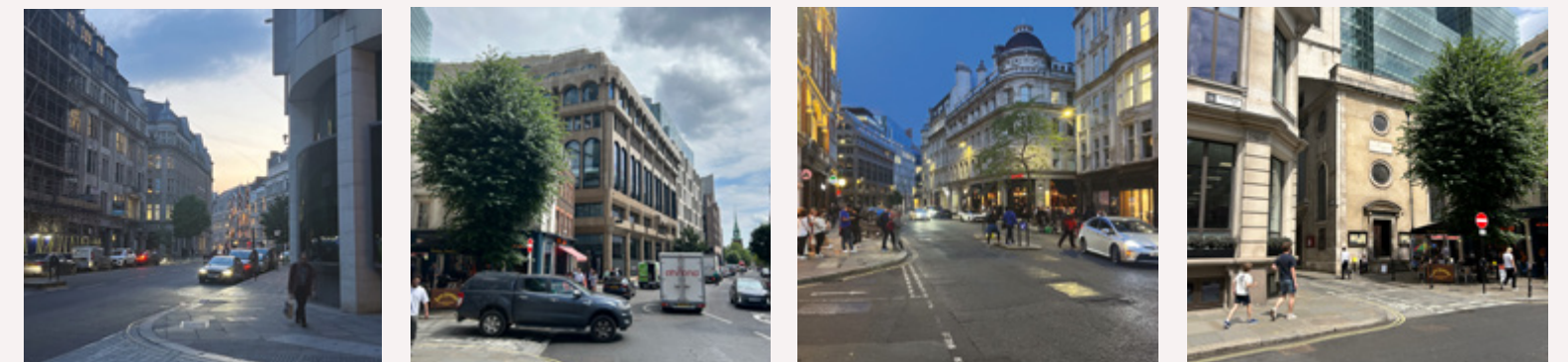
East

Eastcheap

Existing condition

Eastcheap is a busy east-west boulevard connecting Monument to the Tower of London. It is heavily used by vehicles and pedestrians, and would benefit from widened footways, improved pedestrian crossings and cycle facilities. Eastcheap sits within the City of London's Fenchurch Street Healthy Streets Plan. Existing evening uses on Eastcheap create a pocket of after-dark activity anchored around Eastcheap Records, and could be celebrated as a mini cultural cluster.

- 1 Narrow and congested footways
- 2 Delightful architectural features aren't suitably lit and go unnoticed in the evening
- 3 Active use activates St Margaret Patten's forecourt
- 4 Church spire acts as a wayfinding landmark during the day



Eastcheap: A flexible thoroughfare



Widened footways with trees and seating in Sauchiehall Street, Glasgow



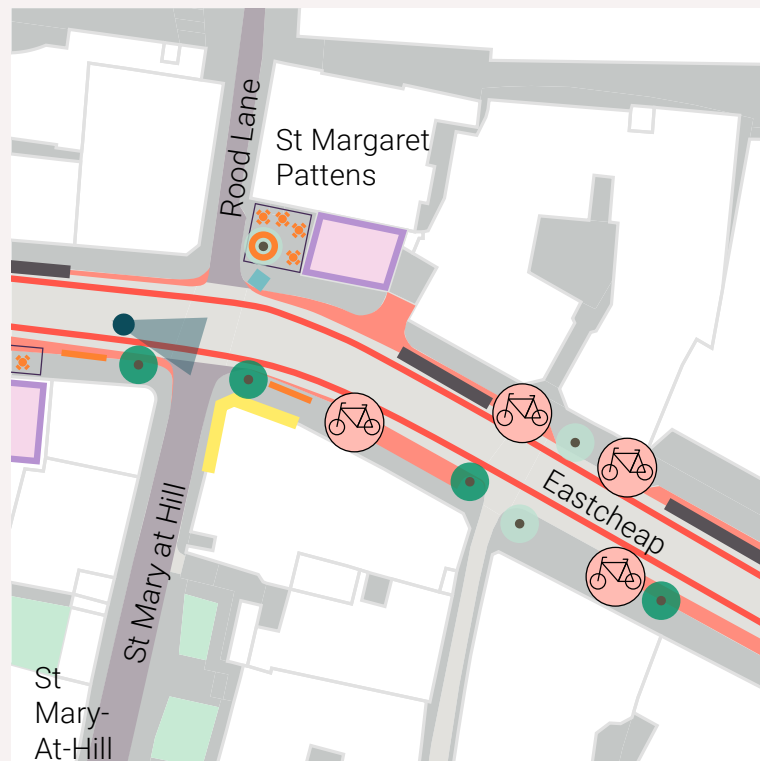
De-cluttered footway with raised loading pad, tree and seating on Marylebone Lane



Cultural events signposted on banners activate the street and contribute to the area's identity

OPPORTUNITIES

- 1 Widened footways provide space for additional seating, planting and cycle parking
- 2 Existing parking retained, consolidated and raised within the footways on both sides of Eastcheap. Timed and shared use bays that provide loading at times that align with the City Plan 2040 and avoid peak pedestrian hours (i.e. no deliveries between 7am–10am, 12pm–2pm and 4pm–7pm on weekdays)
- 3 Opportunity for cultural banners to be installed that celebrate cultural events and contribute to general identity of place
- 4 Lighting to heritage features and notable façades along southern side of the street, echo the existing lighting of façades on the northern elevation
- 5 Additional circular bench to existing feature tree to support active uses
- 6 Lighting to church spire to aid wayfinding after-dark
- 7 Dedicated cycle lanes in each direction (1.7m-wide shown here)



- Existing tree
- Proposed tree
- Active use
- Existing cycle parking
- Paved carriageway
- Wayfinding totem
- Crossing
- Play
- Lighting to heritage façades
- Public seating
- Outdoor dining
- Loading and servicing
- Pavement widening
- Cycle lane
- Viewpoint



Project stakeholders include City of London Corporation, local landowners and property developers, TfL

Overview

At the heart of the Eastern City is a network of historic courts, alleys, churchyards, and buildings that embody the thousands of years of history in this part of the Square Mile. Walking through these alleyways and discovering the small courtyards and historic pubs is one of the joys of this area. The distinctive character of this central block can be enhanced through targeted improvements that align to the vision aims.

RECENT AND UPCOMING PROJECTS

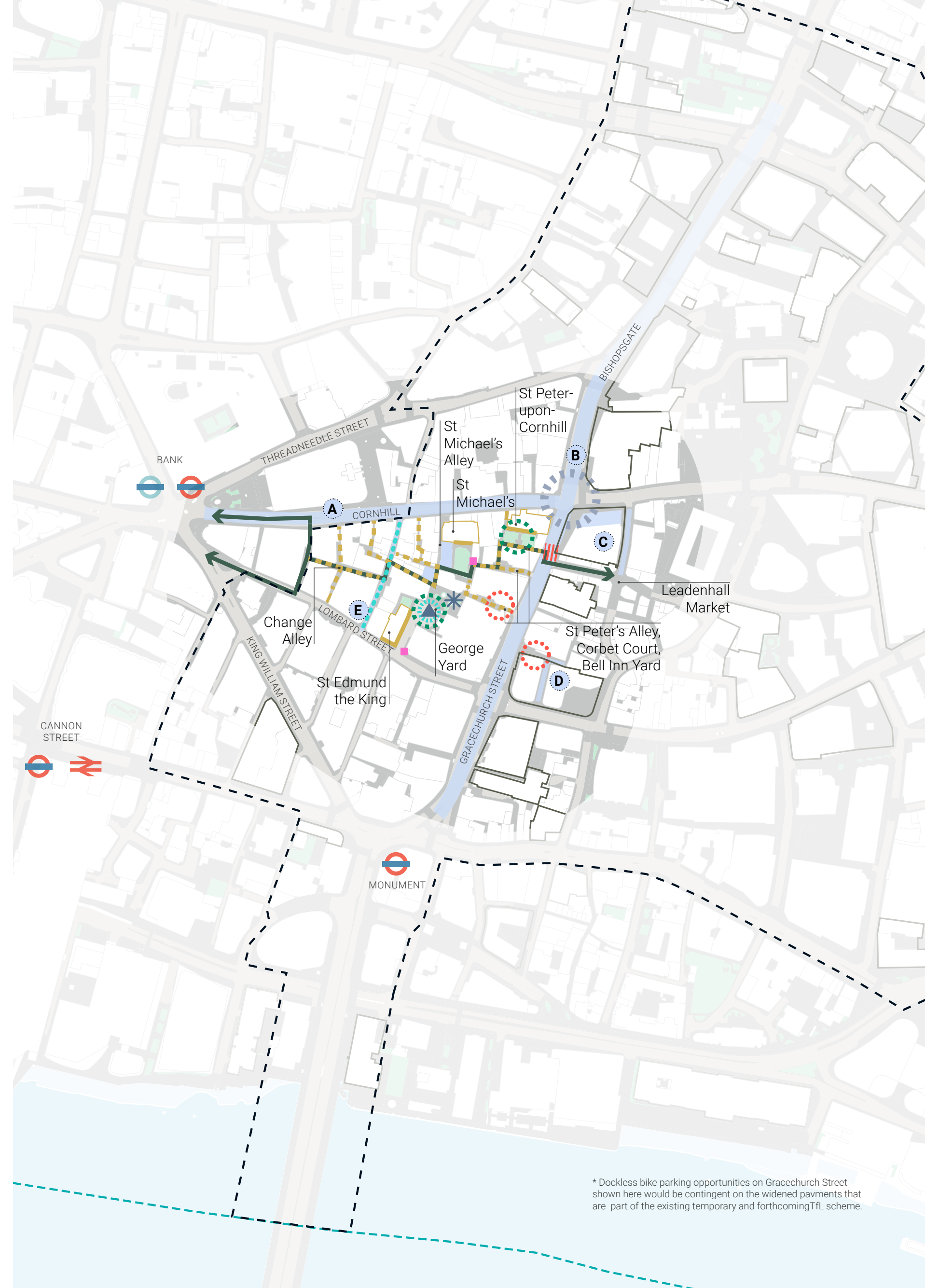
- A** Cornhill traffic scheme (CoL)
- B** Junctions improvements through Monument Junction and Gracechurch/ Bishopsgate schemes (TfL)
- C** New route through 1 Leadenhall Street
- D** New route through 70 Gracechurch Street
- E** Birchin Lane public realm improvements (CoL)
- Development pipeline

PROPOSED IMPROVEMENTS

- New or improved public space Welcoming public space
- Opportunity for publicly-accessible ground floor
- Opportunity for planting Resilient trees and planting
- Space for events Lively uses and activity
- Dressing up the street
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Heritage

- + Development in the area was tied to its proximity to formal areas of trade nearby.
- + Network of alleys and coffee houses were critical sites of informal connection and conversation that supported the nearby institutions.
- + Historic alley pattern is substantially present here, where these dense networks have been erased elsewhere in the BID area. In eastern portion of area, connections to Roman London – scheduled monument in 90 Gracechurch Street and St Peter Upon Cornhill Church (proximity to Roman Forum).



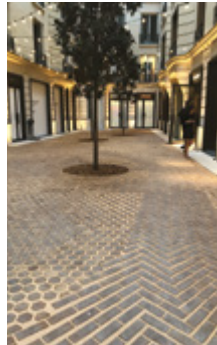
* Dockless bike parking opportunities on Gracechurch Street shown here would be contingent on the widened pavements that are part of the existing temporary and forthcoming TfL scheme.

Bank

Change Alley



Alleyway light installations in Sydney add to the atmosphere of these small spaces after dark.



Patterned paving in BHV yards, Paris, resembling heritage wooden cobbles.



Priority project

Change Alley comprises a network of alleyways lined with active uses, and pedestrian routes connecting Bank to George Yard, St Michael's and St Peter's-upon-Cornhill churchyards. Many of these alleys were occupied by coffee houses, which, as informal sites of information exchange outside the opening hours of the official markets, became crucial to the operation of domestic finances and international trade.

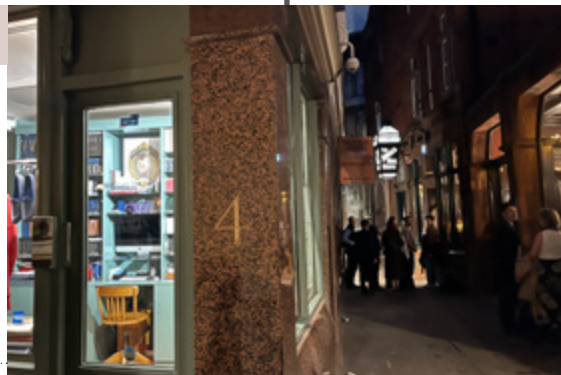
The important role of these coffee houses could be emphasised, by challenging its past and the relationship of these coffee houses with trade of enslaved people. This process of revealing the area's history could help create a more inclusive City.

There is potential for lighting intervention to unveil some heritage features and provide a pleasant outdoor dining experience. Existing cycle parking and dockless cycle parking bays to be retained and enhanced.

St Michael's Alley



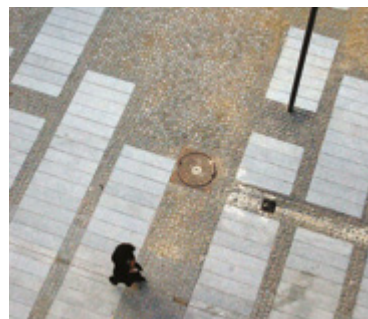
Lantern in Eccleston Yards echoes heritage features and supports wayfinding.



The journey from Change Alley to St Michael's Alley is currently characterised by inactive former shop fronts and uneven lighting. A considered lighting pilot for this network of alleyways could improve the after-dark experience.

Arriving to St Michael's Alley, the scene becomes more animated thanks to the Jamaica Wine Bar and the George and Vulture, two heritage pubs whose guests often spill out into the alleys and St Michael's churchyard.

George Yard



Mix of cobbles and large format paving in Aagaard Landskab, Denmark.



Priority project

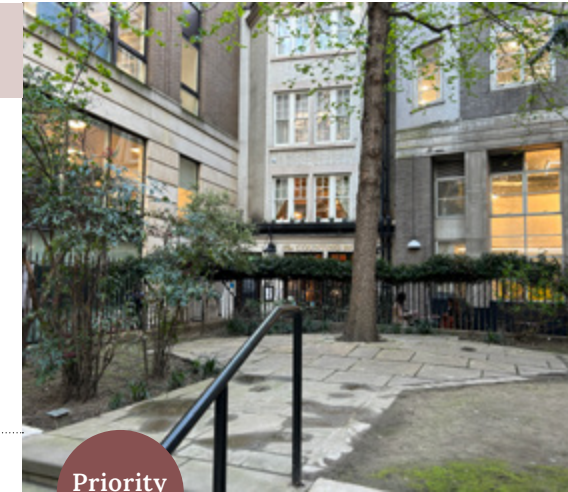
George Yard is a south-facing yard space that comprises a cobbled servicing area with disabled parking spaces, and an adjacent open space, defined by large round vent shafts with integrated planters and benches. There is a view into St Edmund's churchyard, which is often closed but does provide greenery to the setting. It was the site of the Society for Effecting the Abolition of the Slave Trade, which is not acknowledged anywhere in the yard. The space was mentioned in engagement feedback as a potential location for improvements. The nearby pubs often spill out into the open space, which activates it in the early evenings.

Working closely with the adjacent landowners, George Yard could be transformed through careful landscaping that acknowledges the limitations of public realm over a ventilated basement.

St Peter-upon-Cornhill



Edge planting and heritage paving in Temple Inn Courtyard.

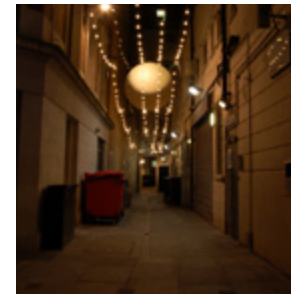


Priority project

This small churchyard sits within the historic location of the Roman forum, placing it at the centre of the oldest part of London. It is currently suffering from some unhealthy planting and the Church has mentioned issues with anti-social behaviour, given the lack of sightlines through the area.

A public realm scheme that provides more resilient planting, permeable paving, and additional seating could drastically improve this cherished open space.

St Peter's Alley, Corbet Court, Bell Inn Yard



Soft lighting and public art installation at Whitcomb Court, Covent Garden.



Connecting St Peter-upon-Cornhill churchyard to Gracechurch Street are a collection of alleyways that each have their own character: Bell Inn Yard is relatively wide, and could even accommodate some seating or public art; Corbet Court sees a fair amount of loading and services, and St Peter's Alley is quiet. Each could benefit from a lighting pilot scheme and some activation with art, murals, or other elements of visual interest that enhance their heritage character.

Gracechurch Street crossing at Leadenhall Market



Raised and paved junction on Bond Street improve pedestrian movement.



The central block of historic alleyways and courtyards is separated from the area's largest heritage asset—Leadenhall Market—by the busy thoroughfare of Gracechurch Street. This means that the entrance to Leadenhall can't be fully celebrated, and the access between these historic locations is disjointed. A pedestrian crossing with localised footway widening could be added here as part of the TfL-led transformation of Gracechurch Street and Bishopsgate.

Leadenhall Market

Next steps

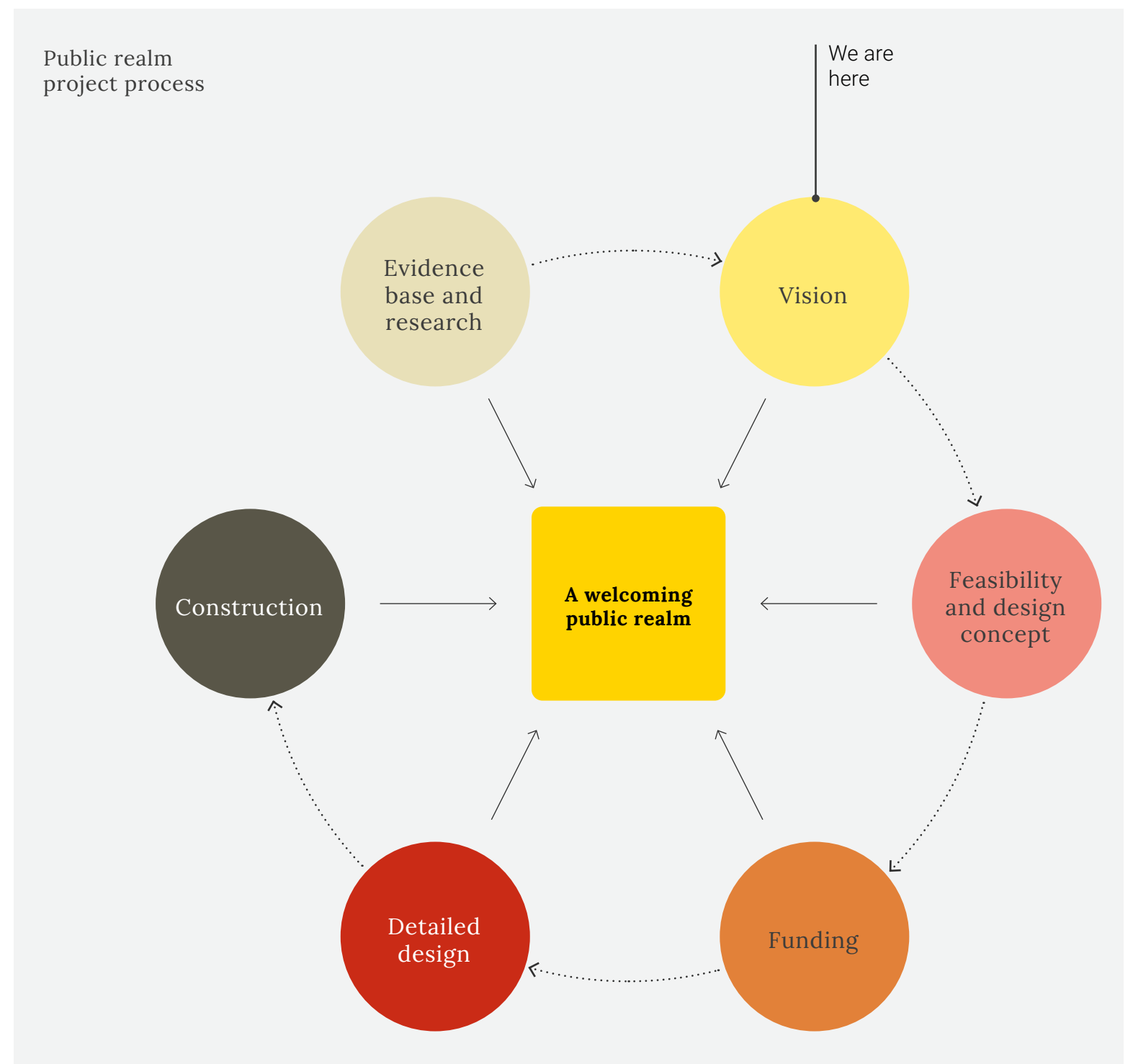
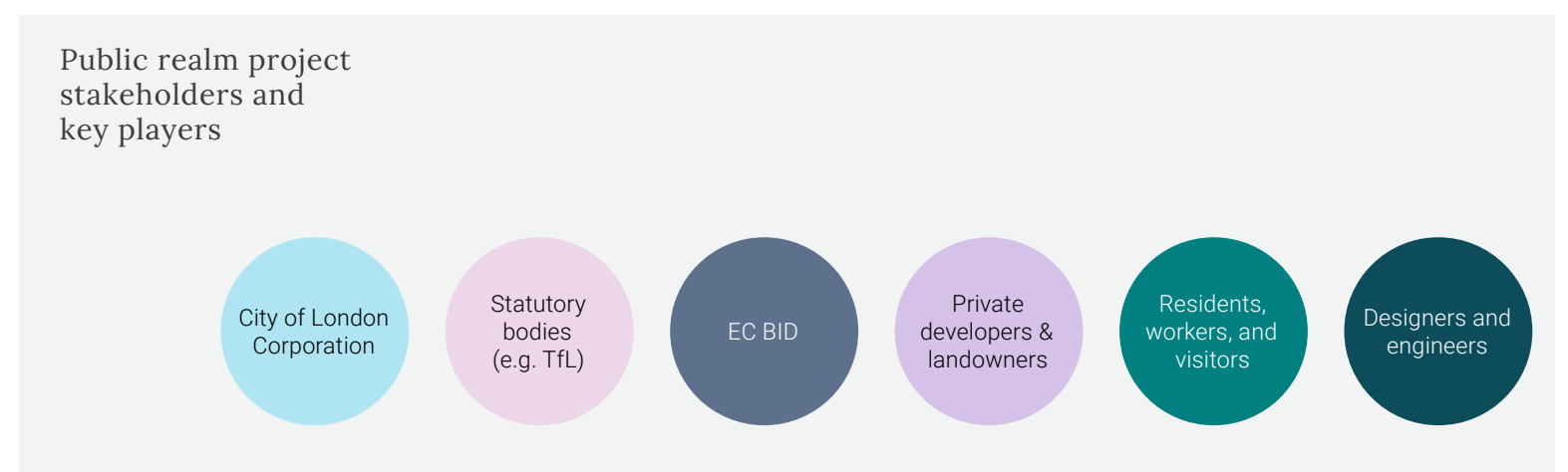
This Public Realm Vision is the first step of a long-term process of collaboration and partnership. Every subsequent step of public realm projects and programmes will be the result of conversations between a broad group of stakeholders, and the BID is eager to play a role in convening these groups and establishing partnerships.

Some of the smaller-scale area-wide projects highlighted in this report, especially those in the Aims and Guiding Principles chapter, can be funded and implemented by the BID directly.

The larger-scale public realm improvements, highlighted as Priority and Transformative Projects, are complex projects requiring ongoing collaboration and partnership across stakeholders.

In the coming months, the BID will work to identify short-term projects to implement, as well as discuss the longer-term transformative projects with public sector partners.

Partner.
Collaborate.
Share knowledge
Convene.
Support.



Thank you

Thank you to all of those who participated in this process by contributing thoughts and time through our pop-ups, online surveys, and one-to-one meetings.

City of London Officers
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Transport for London
City of London Police

St Botolph-without-Bishopsgate
St Mary Abchurch
St Helen's Bishopsgate

Developers and landowners across the BID area

Middlesex Street Estate Tenant and Resident Association

Tower of London
Neighbouring Business Improvement Districts

EC BID Executive Team
EC BID Public Realm Steering Group
EC BID Property Owners Group
EC BID Cultural Partnership Manager

Colophon

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